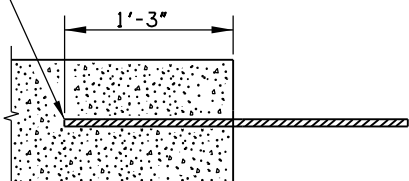


NOTE:  
TIE BARS WILL BE EPOXIED INTO DRILL HOLES WITH SUFFICIENT EPOXY TO COMPLETELY FILL THE VOID BETWEEN THE TIE BAR AND THE HOLE (SEE SECTION 509.04(d)3 OF THE STD. SPECIFICATIONS FOR HIGHWAY CONSTRUCTION, CSCD-5 & PCPR-3). ALL ASSOCIATED COST TO CONSTRUCT CURB, UNLESS OTHERWISE NOTED, SHALL BE INCLUDED IN THE UNIT PRICE BID FOR "P.C. CONCRETE FOR PAVEMENT".

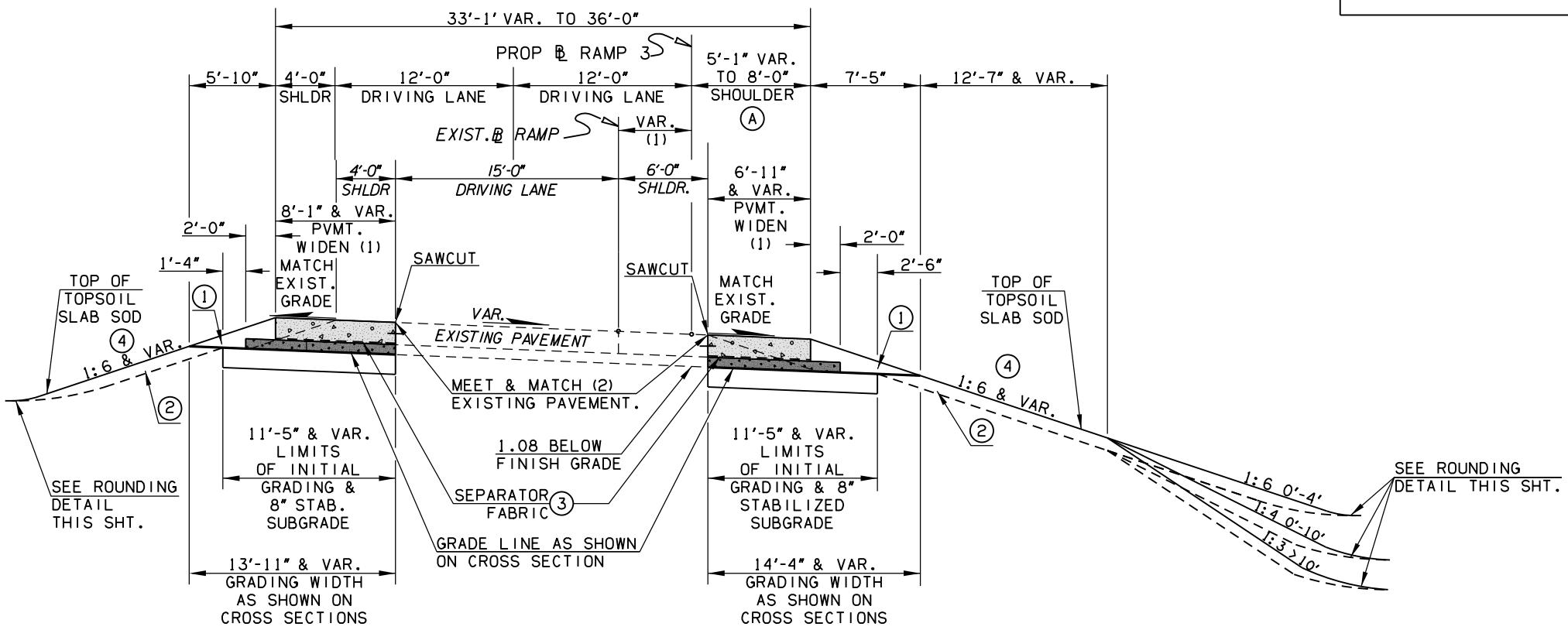
NOTE:  
JOINTS TO BE CLEANED & CONNECTED WITH TIE BARS TO EXISTING CONCRETE PAVING STRUCTURE.

NO. 5 DEF. TIE BAR SPACED @ 3'-0" C/C MAY BE PRE-BENT 90° AND PLACED INTO EDGE OF SLAB AND POSITIONED BEFORE OR DURING SLIP FORM OPERATIONS AND THEN STRAIGHTENED BEFORE PAVING THE ADJACENT PARALLEL LANE. STRAIGHT BARS MAY ALSO BE PLACED THROUGH DRILLED HOLES IN FORMS PRIOR TO PAVING.



**TIED LONGITUDINAL BUTT JOINT**  
(TO BE USED BETWEEN ADJACENT PAVING LANES)

NOTE:  
LONGITUDINAL BUTT JOINT TIE BAR STEEL AND PLACEMENT METHOD SHALL BE APPROVED BY THE ENGINEER.



**TYPICAL RAMP SECTION - LINE 3**

STA. 334+69.22 TO STA. 340+47.77

(1) DISTANCE VARIES SEE CROSS SECTIONS.

(2) THE PROPOSED CONSTRUCTION SHALL MATCH THE EXISTING PAVEMENT BOTH HORIZONTALLY AND VERTICALLY.

PC CONCRETE REQUIREMENT	
13' PAVT. STRUCTURE	
SURFACE COURSE	9.0" DOWEL JOINTED PC CONCRETE
BASE COURSES	4.0" CEMENT TREATED BASE

SEGMENT	WIDTH	EXTENTS
A	TRANS. 5'-1" TO 8'-0"	STA. 335+02.74 TO STA. 336+00.00
A	8'-0"	STA. 336+00.00 TO STA. 340+47.77

① BACKFILL NOTE:  
THIS AREA TO BE BACKFILLED AND COMPACTED AS PART OF THE FINISHING OPERATIONS. QUANTITY IS MEASURED IN UNCLASSIFIED BORROW.

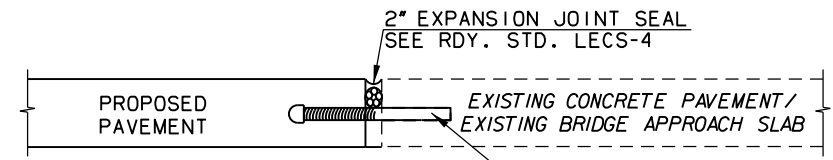
② TOPSOIL NOTE:  
THE CONTRACTOR SHALL STRIP ALL OF THE AVAILABLE TOPSOIL, STOCKPILE IT, AND PLACE IT BACK ON THE SECTION IN ACCORDANCE WITH SECTION 205 OF THE STANDARD SPECIFICATIONS. RESERVED TOPSOIL SHALL BE SPREAD FIRST ON THE COMPLETED SLOPES OF THE CUT SECTIONS AND THE REMAINDER ON COMPLETED FILL SLOPES OR OTHER PRIORITY AREAS LOCATED BY THE ENGINEER. ALL ADDITIONAL COSTS ASSOCIATED WITH OPERATION SHALL BE INCLUDED IN THE PAY ITEM FOR SALVAGED TOPSOIL, LUMP SUM.

THE GRADING LINE AS SHOWN ON THE TYPICAL AND CROSS SECTIONS IS TO THE TOP OF THE TOPSOIL. EARTHWORK QUANTITIES WERE NOT ADJUSTED FOR SALVAGE AND THE TOPSOIL.

③ SEPARATOR FABRIC:  
SHALL BE 15 OZ/S.Y., AS DIRECTED BY SPECIAL PROVISION 317 CEMENT TREATED BASE. COST SHALL BE INCLUDED IN PRICE BID FOR CEMENT TREATED BASE.

④ SEE CROSS SECTIONS.

DIRECTION OF CONSTRUCTION/TRAFFIC

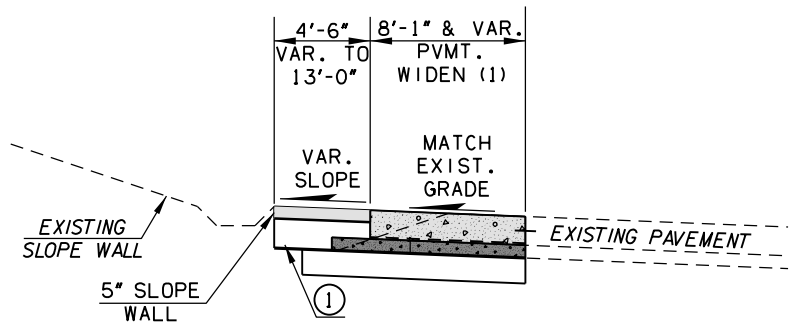
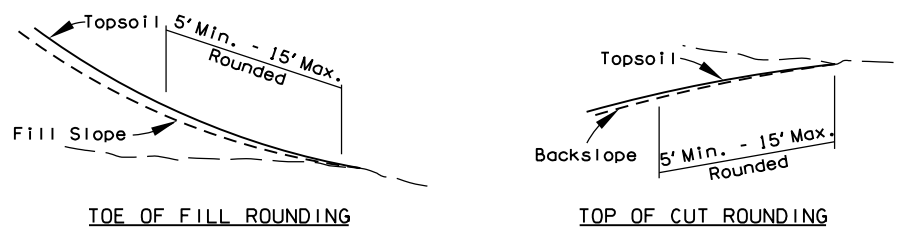


DOWEL BARS SHALL BE EPOXIED (NON-CAPPED END) INTO 1 3/8" DIA. (MAX.) BY 9" DEEP DRILLED HOLES, SPACED AT 1'-0" CTRS., PLACED AT MID-SLAB. DRILLED HOLES AND DOWEL BARS SHALL BE PLACED PARALLEL TO THE DRIVING SURFACE. SUFFICIENT EPOXY SHALL BE USED TO COMPLETELY FILL THE VOID BETWEEN THE DOWEL BAR AND THE HOLE.

**TERMINAL JOINT BETWEEN CONTRACTS**

**ROUNDING DETAIL**

Intersection Of Cut And/Or Fill Slopes With Ground Line To Be Rounded As Part Of Finishing Operations. Rounding Shall Be 5' Minimum For Smaller Cuts And Fills To 15' Maximum For Larger Cuts And Fills Or As Designated By The Engineer. Cost Of Rounding To Be Included In Price Bid For Other Items Of Work.



**DETAIL OF SLOPE WALL "A" CONNECTION**

STA. 338+41.12 TO STA. 340+47.77

(1) DISTANCE VARIES SEE CROSS SECTIONS.

SEE SLOPEWALL DETAIL SHEET NO. 19

DESIGN	-	-
DRAWN	-	-
CHECKED	-	-
APPROVED	-	-
SQUAD	POE	

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