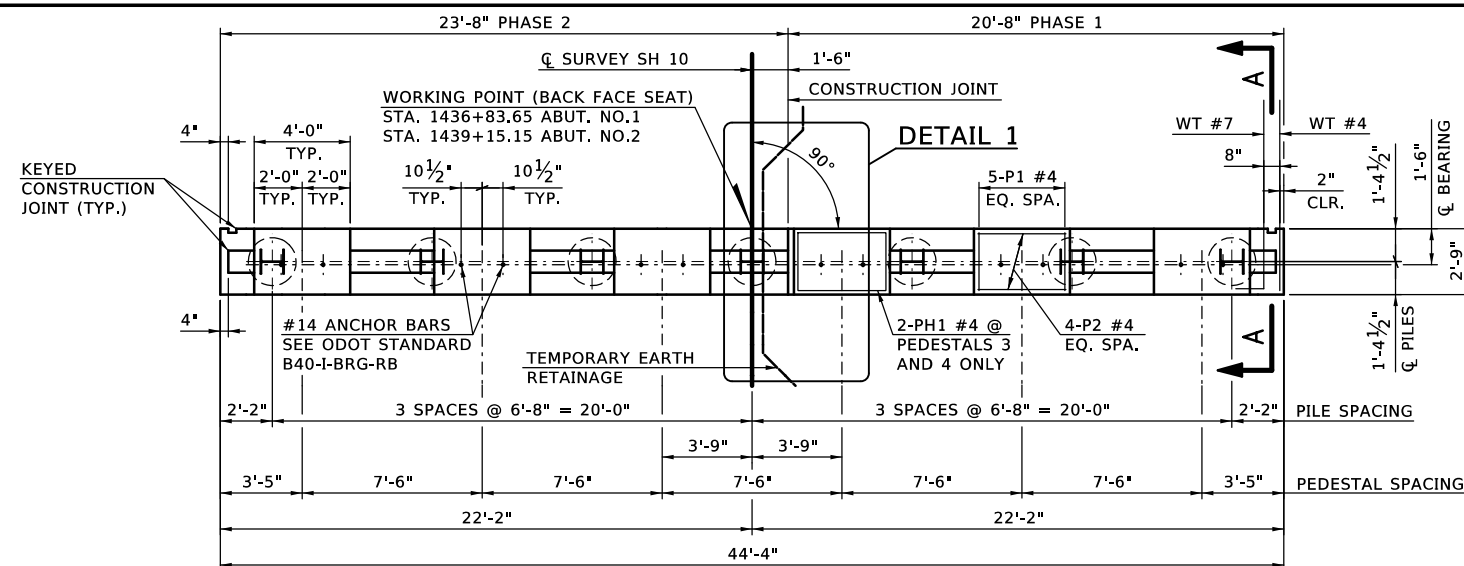
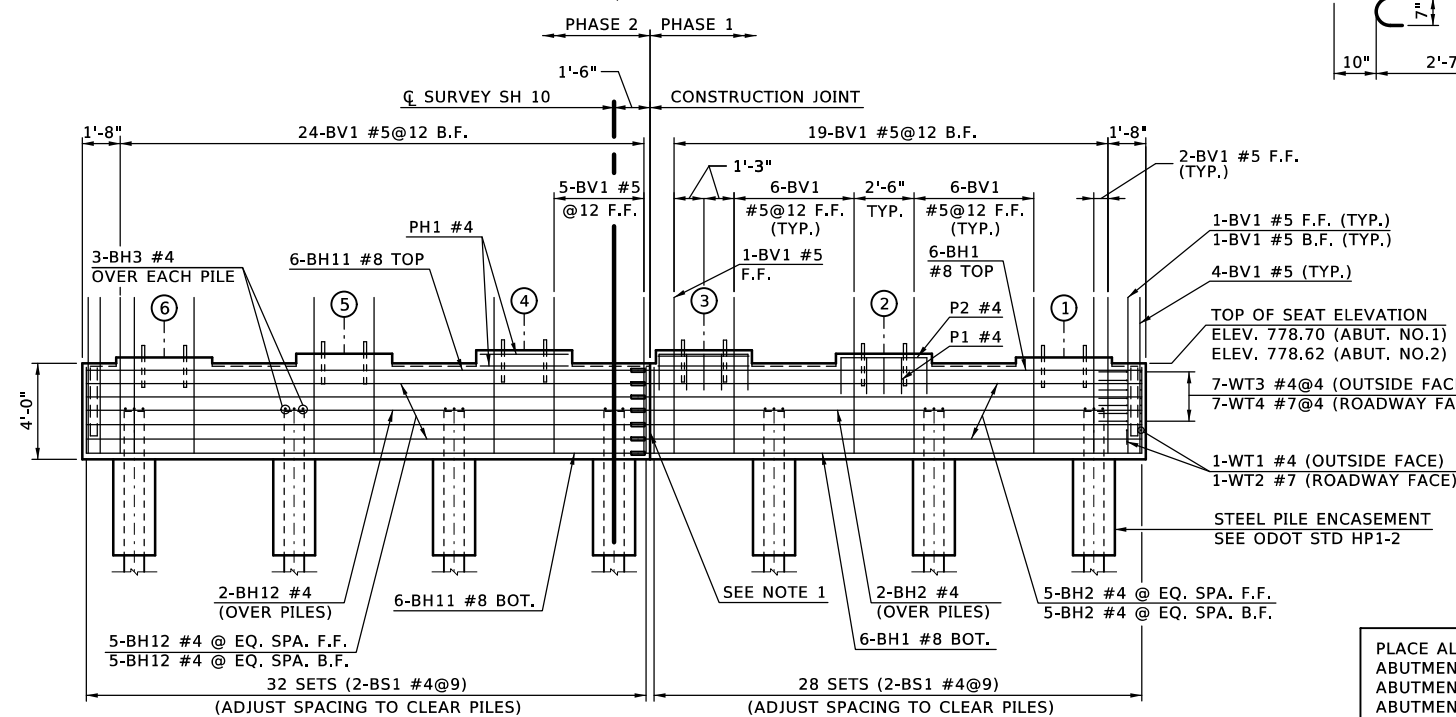


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**PLAN - ABUTMENT NO. 1 AND NO. 2**

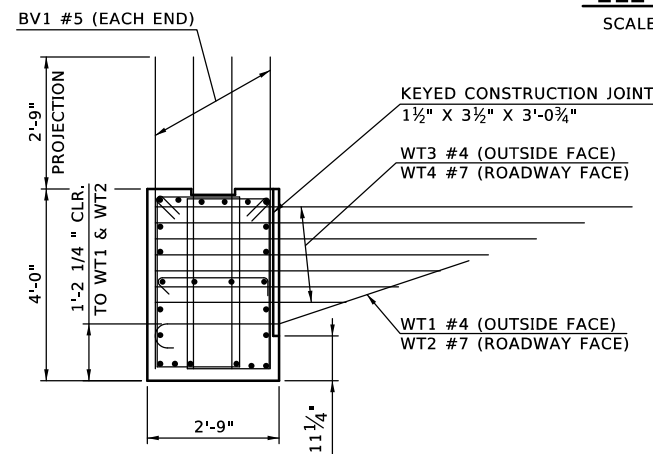
SCALE: 1/4" = 1'-0"



ABUTMENT NO. 2 LOOKING UP STATION  
ABUTMENT NO. 1 LOOKING DOWN STATION OPP. HAND

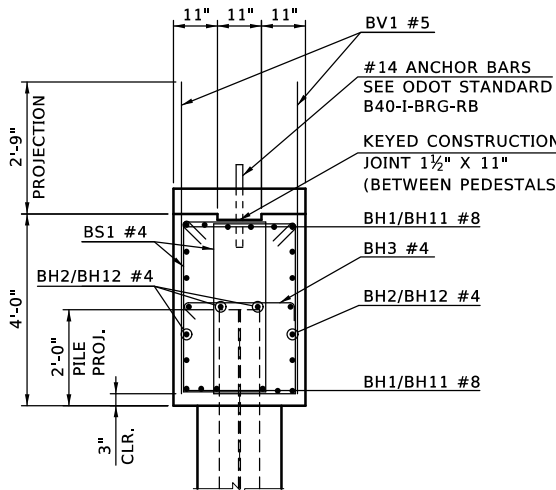
**ELEVATION**

SCALE: 1/4" = 1'-0"



**SECTION A-A**

SCALE: 1/2" = 1'-0"



**TYPICAL SECTION THRU SEAT**

SCALE: 1/2" = 1'-0"

PLACE ALL WT WING REINFORCING TIED TO ABUTMENT SEAT REINFORCING BEFORE PLACING ABUTMENT SEAT CONCRETE. DO NOT PLACE ABUTMENT WING CONCRETE UNTIL CONCRETE FOR THE ABUTMENT DIAPHRAGM AND DECK SLAB HAVE ATTAINED A STRENGTH OF 3000 P.S.I.

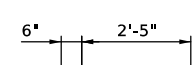
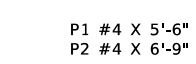
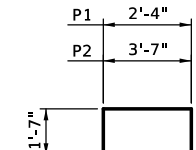
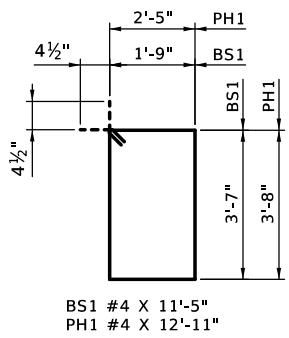
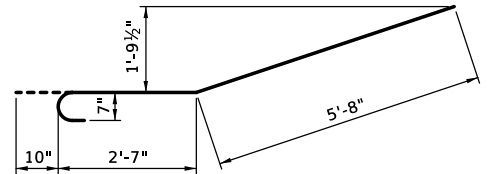
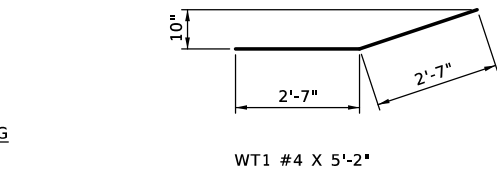
**TOP OF PEDESTAL ELEVATIONS**

PEDESTAL	ABUT. NO. 1	ABUT. NO. 2
1	778.95	778.87
2	779.10	779.02
3	779.25	779.17
4	779.25	779.17
5	779.10	779.02
6	778.95	778.87

NOTE1: PLACE A 6" X 3/16" SERRATED WATERSTOP ALONG FULL HEIGHT OF ABUTMENT SEAT AND DIAPHRAGM AT CONSTRUCTION JOINT.

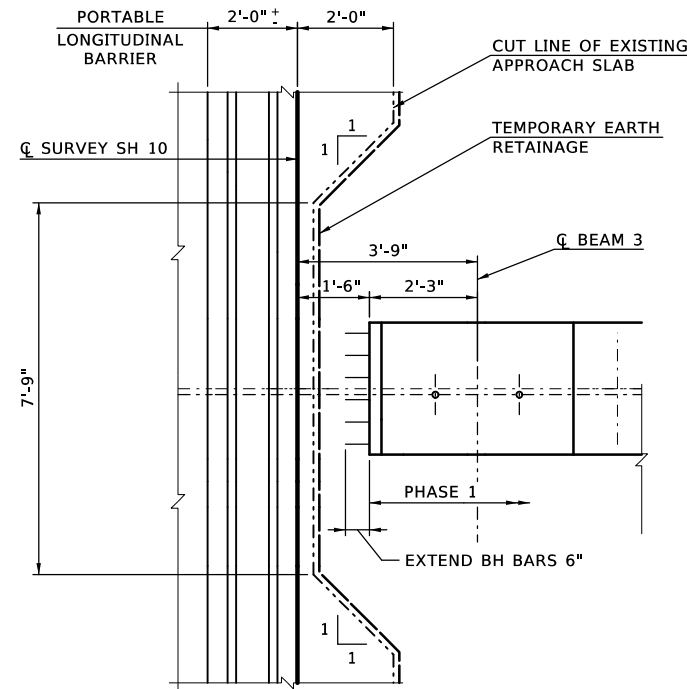
F.F. = FRONT FACE  
B.F. = BACK FACE

SEE SHEET 40 FOR KEY PLAN FOR PHASING



ABUTMENT BAR LIST - PHASE 1				
(ONE SHOWN, TWO REQUIRED)				
EPOXY COATED REINFORCING				
MARK	SIZE	QTY.	FORM	LENGTH
BV1	#5	40	STR	6'-6"
BH1	#8	12	STR	21'-0"
BH2	#4	12	STR	21'-0"
BH3	#4	9	BNT	3'-7"
BS1	#4	56	BNT	11'-5"
WT1	#4	1	BNT	5'-2"
WT2	#7	1	BNT	9'-1"
WT3	#4	7	STR	7'-5" (AVG.)
WT4	#7	7	BNT	8'-7" (AVG.)
P1	#4	15	BNT	5'-6"
P2	#4	12	BNT	6'-9"
PH1	#4	2	BNT	12'-11"

ABUTMENT BAR LIST - PHASE 2				
(ONE SHOWN, TWO REQUIRED)				
EPOXY COATED REINFORCING				
MARK	SIZE	QTY.	FORM	LENGTH
BV1	#5	49	STR	6'-6"
BH3	#4	12	BNT	3'-7"
BH11	#8	12	STR	23'-0"
BH12	#4	12	STR	23'-0"
BS1	#4	64	BNT	11'-5"
WT1	#4	1	BNT	5'-2"
WT2	#7	1	BNT	9'-1"
WT3	#4	7	STR	7'-5" (AVG.)
WT4	#7	7	BNT	8'-7" (AVG.)
P1	#4	15	BNT	5'-6"
P2	#4	12	BNT	6'-9"
PH1	#4	2	BNT	12'-11"



**DETAIL 1**

SCALE: 1/2" = 1'-0"

Design	LJW	7/16	SH 10 OVER BIG CABIN CREEK	CRAIG COUNTY
Drawn	RAH	7/16	BRIDGE A	
Checked	KSJ	7/16	ABUTMENT DETAILS (1)	
Approved	SAK	9/16		
Squad	BENHAM		Job Piece No. 29068(04)	Sheet No. 38