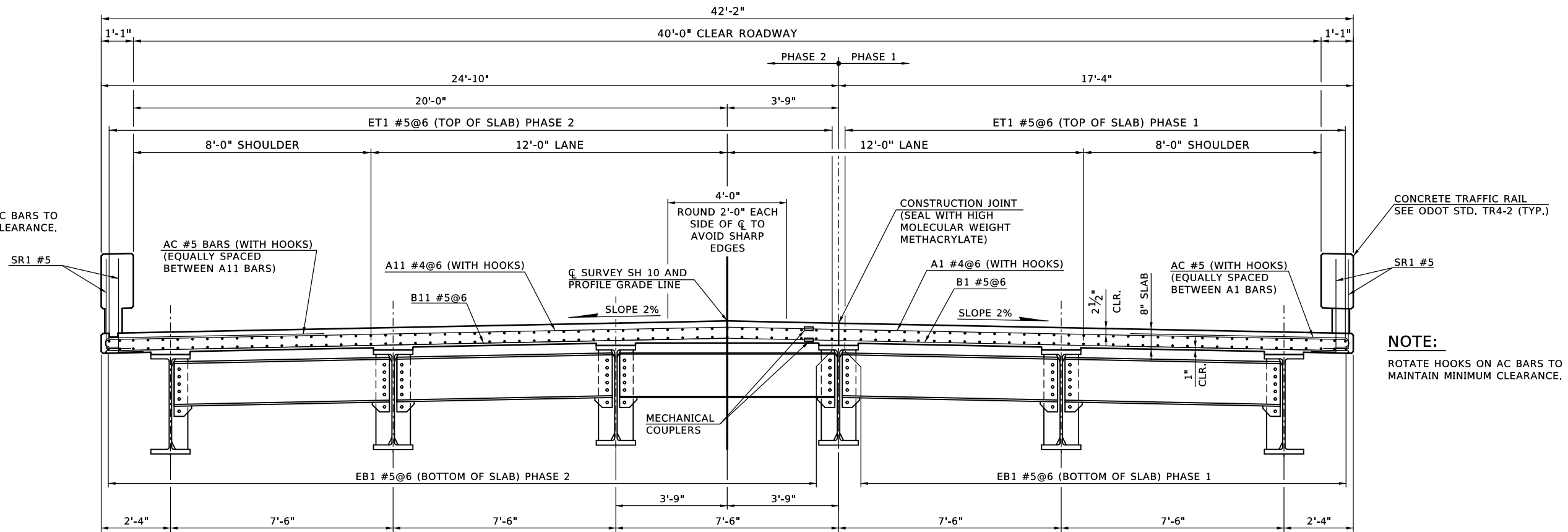


NOTE:
ROTATE HOOKS ON AC BARS TO MAINTAIN MINIMUM CLEARANCE.



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ROTATE HOOKS ON AC BARS TO MAINTAIN MINIMUM CLEARANCE.

SPAN 2 SHOWN (SPAN 1 AND 3 SIMILAR)
TYPICAL SECTION AT INTERMEDIATE DIAPHRAGM

THE DECK THICKNESS IS INCREASED 1/4" TO ALLOW FOR ADDITIONAL GRINDING IN THE LANE AND ADJACENT LANES WHERE THE LONGITUDINAL CONSTRUCTION JOINT IS LOCATED. THE DECK THICKNESS SHALL BE 8 1/4" THICK AT THE LONGITUDINAL JOINT LOCATION AND TRANSITION TO 8" THICK AT THE ADJACENT BEAM LOCATED 7'-6" FROM THE JOINT (PHASE 1) AND AT THE BRIDGE PROFILE GRADE LINE (PHASE 2). GRINDING SHALL BE PERFORMED AS DIRECTED BY THE ENGINEER AFTER ANY REQUIRED SMOOTHNESS CORRECTIONS ARE DONE TO MEET REQUIREMENTS OF SP 430 PAVEMENT SMOOTHNESS. THE INTENT IS TO CORRECT ROUGHNESS AT THE LONGITUDINAL JOINT DUE TO PAVEMENT FINISHING OPERATIONS. GRINDING ADJACENT LANES MAY ALSO BE REQUIRED TO FEATHER OUT AND REMOVE GRINDING DEPTHS GREATER THAN 1/8". ALL COST OF GRINDING WILL BE INCLUDED IN THE COST OF THE CLASS AA CONCRETE PAY ITEM USED IN THE BRIDGE DECK.

Design	MKR	7/16	SH 10 OVER BIG CABIN CREEK	CRAIG COUNTY
Drawn	JT	7/16	BRIDGE A	
Checked	KSJ	7/16	TYPICAL SECTION (2)	
Approved	SAK	9/16		
Squad	BENHAM		Job Piece No. 29068(04)	Sheet No. 35