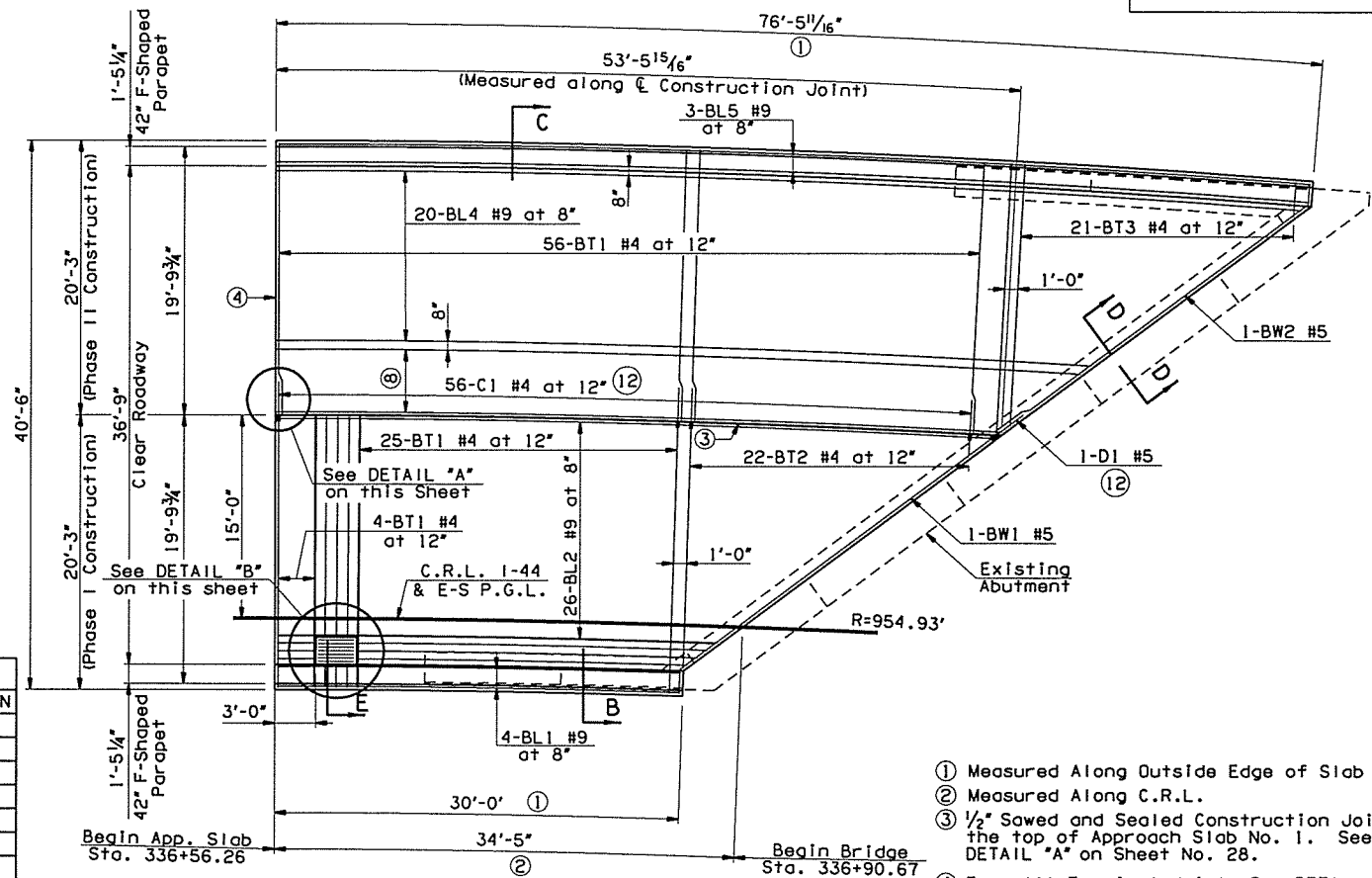
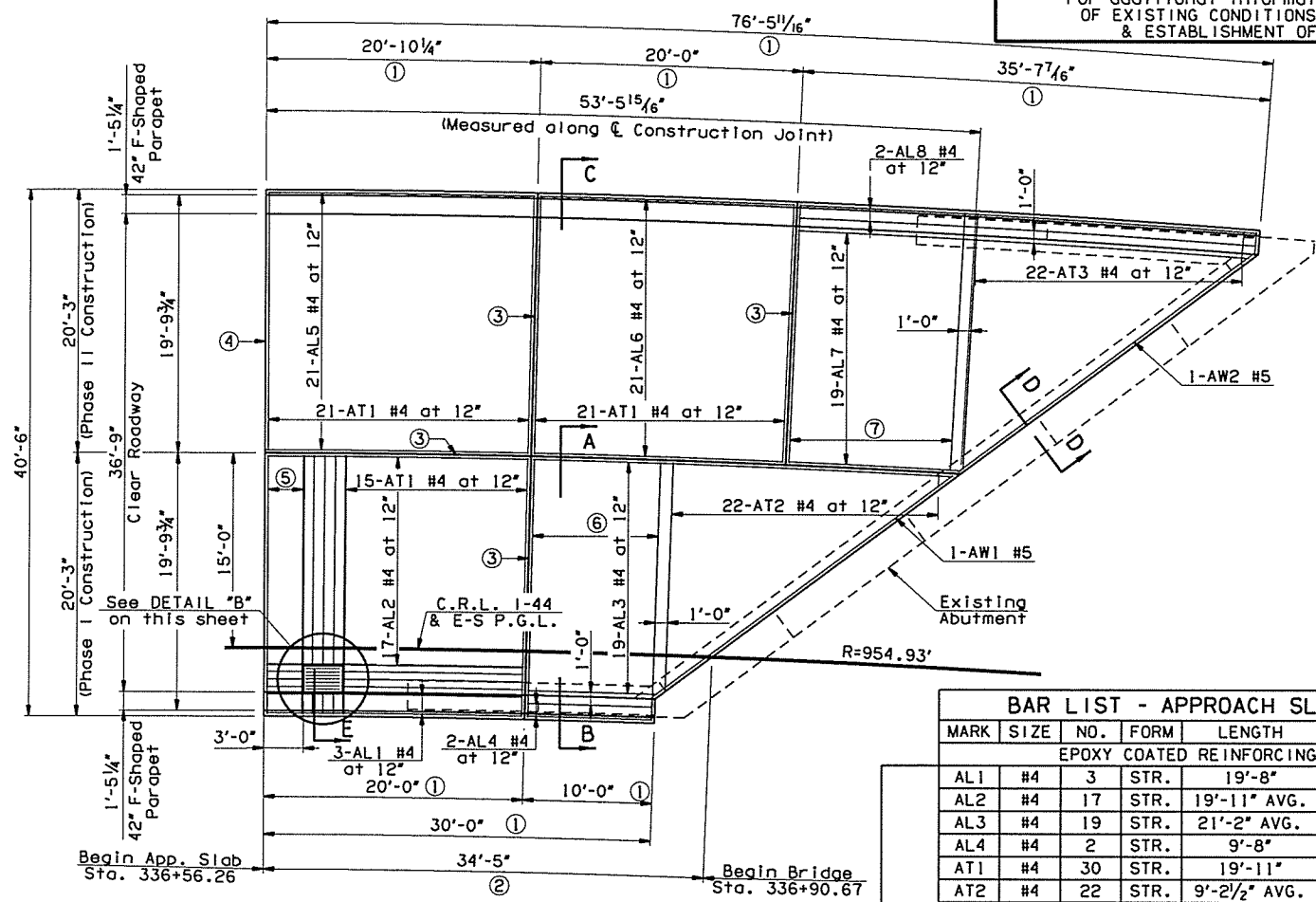


All information included in these plans is based on the existing As-Surveyed data. It is solely the Contractor's responsibility to accurately verify this information prior to any demolition or construction. For additional information, see the General Notes "VERIFICATION OF EXISTING CONDITIONS", "SURVEYING AND CONSTRUCTION STAKING", & ESTABLISHMENT OF VERTICAL GEOMETRY" on Sheet No. 3.

OKLAHOMA DEPARTMENT OF TRANSPORTATION					
FED. ROAD DIST. NO.	STATE	JOB PIECE NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	29775(04)			
DESCRIPTION			REVISIONS		DATE



APPROACH SLAB AT ABUTMENT NO. 1
(Showing Top Mat of Reinforcing Steel)

APPROACH SLAB AT ABUTMENT NO. 1
(Showing Bottom Mat of Reinforcing Steel)

BAR LIST - APPROACH SLAB NO. 1

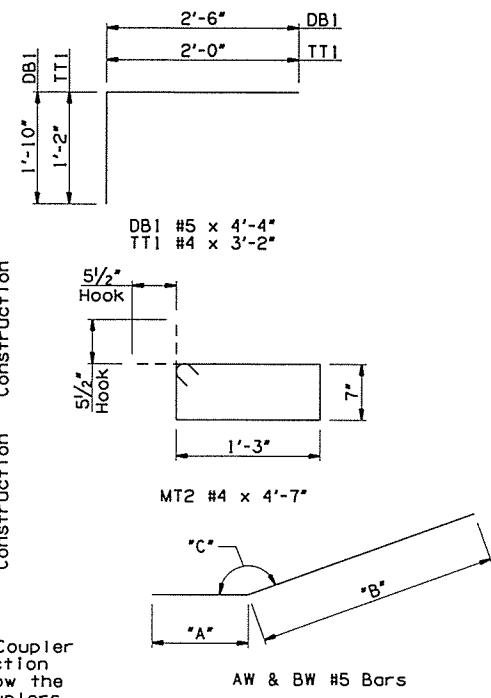
MARK	SIZE	NO.	FORM	LENGTH	LENGTH VARIATION
EPOXY COATED REINFORCING STEEL					
AL1	#4	3	STR.	19'-8"	-
AL2	#4	17	STR.	19'-11" AVG.	19'-9" TO 20'-1"
AL3	#4	19	STR.	21'-2" AVG.	10'-0" TO 32'-4"
AL4	#4	2	STR.	9'-8"	-
AT1	#4	30	STR.	19'-11"	-
AT2	#4	22	STR.	9'-2 1/2" AVG.	1'-0" TO 17'-5"
AW1	#5	1	BENT	30'-6"	-
BL1	#9	4	STR.	29'-8"	-
BL2	#9	26	STR.	42'-7" AVG.	32'-5" TO 52'-9"
BT1	#4	29	STR.	20'-1"	-
BT2	#4	22	STR.	9'-11" AVG.	1'-8" TO 18'-2"
BW1	#5	1	BENT	30'-10"	-
DB1	#5	21	BENT	4'-4"	-
FS2	#5	32	BENT	7'-4"	-
ML1	#4	3	STR.	2'-5"	-
ML2	#4	3	STR.	13'-5"	-
ML3	#4	2	STR.	31'-0" AVG.	29'-8" TO 32'-4"
MT1	#4	3	STR.	15'-8"	-
MT2	#4	5	BENT	4'-7"	-
MT3	#4	2	STR.	19'-11"	-
NL1	#9	3	STR.	2'-5"	-
NL2	#9	3	STR.	24'-8" AVG.	23'-11" TO 25'-5"
NT1	#4	3	STR.	15'-10"	-
NT2	#4	2	STR.	20'-1"	-
TT1	#4	33	BENT	3'-2"	-
TT2	#4	1	STR.	29'-8"	-
AL5	#4	21	STR.	20'-4 1/2" AVG.	20'-3" TO 20'-6"
AL6	#4	21	STR.	19'-5 1/2" AVG.	19'-3" TO 19'-8"
AL7	#4	19	STR.	25'-6" AVG.	13'-4" TO 37'-8"
AL8	#4	2	STR.	35'-3"	-
AT1	#4	56	STR.	19'-11"	-
AT3	#4	22	STR.	11'-1 1/2" AVG.	19'-9" TO 2'-6"
AW2	#5	1	BENT	29'-10"	-
BL3	#9	8	STR.	56'-5" AVG.	53'-6" TO 59'-4"
BL4	#9	20	STR.	75'-11" AVG.	68'-2" TO 83'-8"
BL5	#9	3	STR.	84'-2"	-
BT1	#4	56	STR.	20'-1"	-
BT3	#4	21	STR.	10'-11" AVG.	2'-6" TO 19'-4"
BW2	#5	1	BENT	30'-2"	-
C1	#4	56	STR.	2'-6"	-
D1	#5	1	STR.	3'-0"	-
DB1	#5	21	BENT	4'-4"	-
FS2	#5	78	BENT	7'-4"	-
TT1	#4	78	BENT	3'-2"	-
TT3	#4	1	STR.	78'-8"	-

PHASE I CONST. (1) (2) (3) (4) (5) (6) (7) (8) (9) (10) (11) (12) (13) (14)

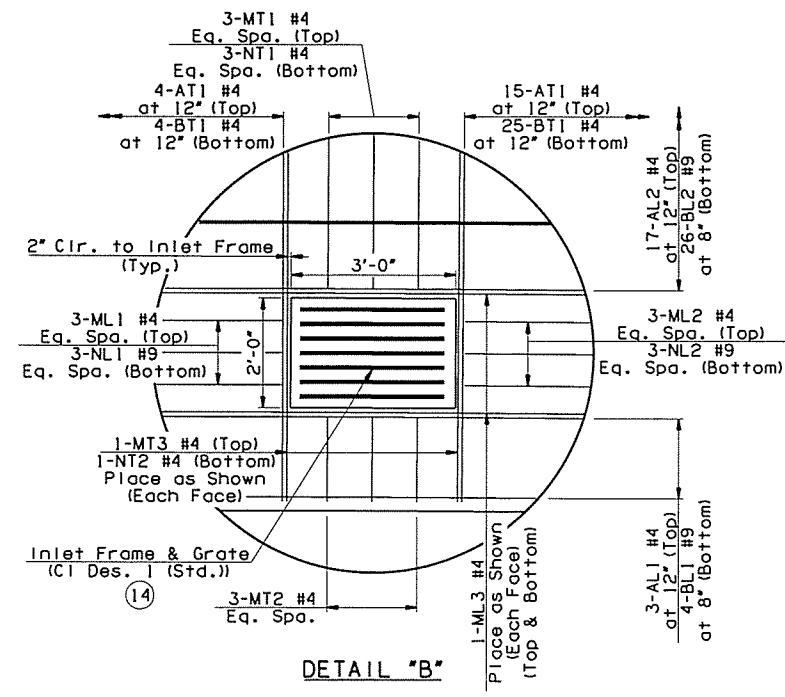
PHASE II CONST. (1) (2) (3) (4) (5) (6) (7) (8) (9) (10) (11) (12) (13) (14)

- ① Measured Along Outside Edge of Slab
- ② Measured Along C.R.L.
- ③ 1/2" Sawn and Sealed Construction Joint in the top of Approach Slab No. 1. See DETAIL "A" on Sheet No. 28.
- ④ Type III Terminal Joint. See DETAIL "A" on Sheet No. 41.
- ⑤ 4-AT1 #4 at 12"
- ⑥ 11-AT1 #4 at 12"
- ⑦ 14-AT1 #4 at 12"
- ⑧ 8-BL3 #9 at 8"
- ⑨ Includes One 8'-0" Lap Length.
- ⑩ Includes One 2'-6" Lap Length.
- ⑪ 2 Sets of 2 bars.
- ⑫ Install new Inlet Frame, Inlet Grate, Inlet & 18" RCP (to connect to existing 18" RCP). All costs including materials, labor, equipment and incidentals shall be included in the price bid per Lump Sum of "IPL" INSTALLATION OF BRIDGE ITEMS". See Stds. SSIF-4-0 & CI-1-2 for details.

NOTES:
Do not groove within 6" of any Longitudinal Joint or Construction Joint between the Approach Slab and the Deck Slab.
All transverse reinforcement shall be placed along radial lines to C.R.L. and are measured along edge of slab.
All longitudinal reinforcement shall be oriented along a curve concentric with C.R.L.
Parapet reinforcement not shown for clarity. See Sheet No. 35 for details.
See Sheet No. 41 for SECTIONS A, B, C, D-D & E and Summary of Quantities.
For Approach Slab No. 2 details, see Sheet No. 40.



MARK	"A"	"B"	"C"	TOTAL
AW1	1'-10"	28'-8"	129°	30'-6"
AW2	1'-7"	28'-3"	131°	29'-10"
BW1	1'-10"	29'-0"	129°	30'-10"
BW2	1'-7"	28'-7"	131°	30'-2"



- ⑫ Install Mechanical Reinforcing Bar Coupler (Epoxy Coated) in accordance with Section 511.04.C.3. Installation shall follow the Manufacturer's recommendations. Couplers shall be attached to: BT1, BT2, NT1, NT2 #4 & BW1 #5.
- ⑬ Splice the following bars: BT1, BT2, NT1 & NT2 Bars with C1 Bars; BW1 Bars with D1 Bars; Lap the following bars: C1 Bars with BT1 Bars; D1 Bars with BW2 Bars

I-44 OVER I-244 NB BRIDGE "A"		TULSA COUNTY		DESIGN	JTR	4/15
				DETAIL	SJL	5/16
				CHECK	BRT	5/16
				GARVER		
STATE OF OKLAHOMA	DEPARTMENT OF TRANSPORTATION			JOB PIECE NO. 29775(04) SHEET NO. 39		