

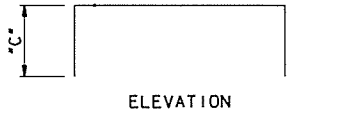
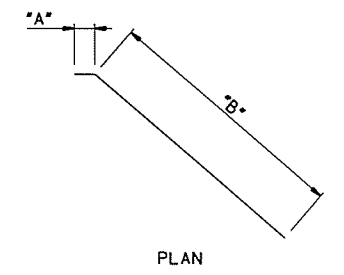
All information included in these plans is based on the existing As-Surveyed data. It is solely the Contractor's responsibility to accurately verify this information prior to any demolition or construction. For additional information, see the General Notes "VERIFICATION OF EXISTING CONDITIONS", "SURVEYING AND CONSTRUCTION STAKING", & ESTABLISHMENT OF VERTICAL GEOMETRY" on Sheet No. 3.

OKLAHOMA DEPARTMENT OF TRANSPORTATION						
FED. ROAD DIST. NO.	STATE	JOB PIECE NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS	
6	OKLA.	29775(04)				
DESCRIPTION						DATE

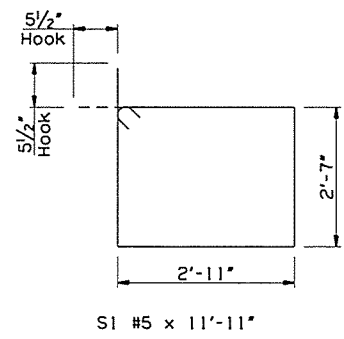
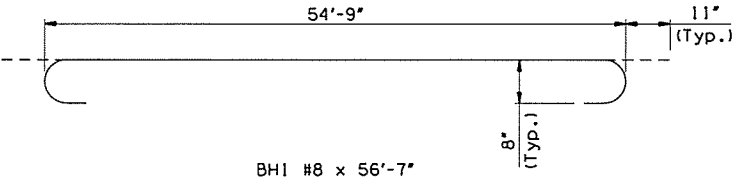
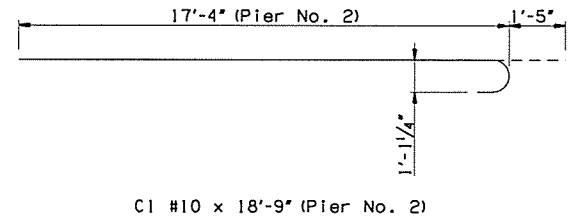
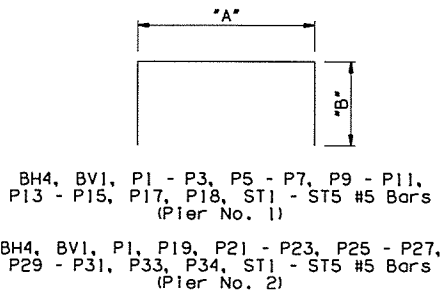
SUMMARY OF QUANTITIES - PIERS				
ITEM	UNIT	PIER NO. 1	PIER NO. 2	TOTAL
SPECIAL CONCRETE FINISH	S.Y.	45.00	46.00	91.00
CLASS A CONCRETE	C.Y.	24.30	28.50	52.80
REINFORCING STEEL	LB.		181.00	181.00
EPOXY COATED REINFORCING STEEL	LB.	4,906.00	5,702.00	10,608.00
WATER REPELLENT (VISUALLY INSPECTED)	S.Y.	22.90	22.90	45.80
PREPARATION OF CRACKS, ABOVE WATER	L.F.	24.00	24.00	48.00
EPOXY RESIN, ABOVE WATER	GAL.	0.80	0.80	1.60
PNEUMATICALLY PLACED MORTAR	S.Y.	1.40	1.40	2.80
(PL) REPAIR BRIDGE ITEMS	S.Y.	0.70	0.70	1.40

BAR LIST - PIER NO. 1							
MARK	SIZE	NO.	FORM	A	B	C	LENGTH
EPOXY COATED REINFORCING STEEL							
BH1	#8	11	BENT				56'-7"
BH2	#8	6	STR.				54'-9"
BH3	#5	4	STR.				54'-9"
BH4	#4	4	BENT	3'-0"	1'-0"		5'-0"
BV1	#4	6	BENT	2'-7"	1'-0"		4'-7"
P1	#5	9	BENT	3'-0"	1'-8"		6'-4"
P2	#5	1	BENT	2'-2"	1'-8"		5'-6"
P3	#5	1	BENT	1'-2"	1'-8"		4'-6"
P4	#5	1	BENT	3"	3'-10"	1'-6"	7'-1"
P5	#5	4	BENT	2'-8" TO 4'-8"	1'-8"		7'-0" AVG. 6'-0" TO 8'-0"
P6	#5	1	BENT	5'-1"	1'-8"		8'-5"
P7	#5	6	BENT	1'-0" TO 2'-8"	1'-8"		5'-2" AVG. 4'-4" TO 6'-0"
P8	#5	2	BENT	3"	4'-5"	1'-6"	7'-8"
P9	#5	8	BENT	6" TO 3'-3"	1'-8"		5'-2 1/2" AVG. 3'-10" TO 6'-7"
P10	#5	2	BENT	3'-9"	1'-8"		7'-1"
P11	#5	3	BENT	1'-2" TO 3'-0"	1'-8"		5'-5" AVG. 4'-6" TO 6'-4"
P12	#5	1	BENT	3"	3'-10"	1'-6"	7'-1"
P13	#5	4	BENT	1'-6" TO 3'-6"	1'-8"		5'-10" AVG. 4'-10" TO 6'-10"
P14	#5	1	BENT	3'-11"	1'-8"		7'-3"
P15	#5	3	BENT	1'-1" TO 2'-9"	1'-8"		5'-3" AVG. 4'-5" TO 6'-1"
P16	#5	1	BENT	3"	3'-10"	1'-6"	7'-1"
P17	#5	4	BENT	6" TO 2'-5"	1'-8"		4'-9 1/2" AVG. 3'-10" TO 5'-9"
P18	#5	1	BENT	2'-10"	1'-8"		6'-2"
S1	#5	76	BENT				11'-11"
ST1	#5	70	BENT	3'-2"	1'-6"		6'-2"
ST2	#5	10	BENT	6'-8"	1'-6"		9'-8"
ST3	#5	5	BENT	6'-10"	1'-6"		9'-10"
ST4	#5	15	BENT	8'-0"	1'-6"		11'-0"
ST5	#5	5	BENT	8'-4"	1'-6"		11'-4"

BAR LIST - PIER NO. 2							
MARK	SIZE	NO.	FORM	A	B	C	LENGTH
EPOXY COATED REINFORCING STEEL							
BH1	#8	11	BENT				56'-7"
BH2	#8	6	STR.				54'-9"
BH3	#5	4	STR.				54'-9"
BH4	#4	4	BENT	3'-0"	1'-0"		5'-0"
BV1	#4	6	BENT	2'-7"	1'-0"		4'-7"
C1	#10	10	BENT				18'-9"
P1	#5	8	BENT	3'-0"	1'-8"		6'-4"
P19	#5	3	BENT	11" TO 2'-6"	1'-6"		4'-8 1/2" AVG. 3'-11" TO 5'-6"
P20	#5	1	BENT	3"	4'-4"	1'-6"	7'-7"
P21	#5	1	BENT	3'-3"	1'-6"		6'-3"
P22	#5	4	BENT	6" TO 2'-9"	1'-6"		4'-7 1/2" AVG. 3'-6" TO 5'-9"
P23	#5	6	BENT	1'-1" TO 3'-0"	1'-6"		5'-0 1/2" AVG. 4'-1" TO 6'-0"
P24	#5	2	BENT	3"	4'-2"	1'-6"	7'-5"
P25	#5	2	BENT	3'-9"	1'-6"		6'-9"
P26	#5	8	BENT	9" TO 3'-3"	1'-6"		5'-0" AVG. 3'-9" TO 6'-3"
P27	#5	3	BENT	1'-0" TO 2'-8"	1'-6"		4'-10" AVG. 4'-0" TO 5'-8"
P28	#5	1	BENT	3"	4'-4"	1'-6"	7'-7"
P29	#5	1	BENT	4'-4"	1'-6"		7'-4"
P30	#5	4	BENT	1'-3" TO 3'-10"	1'-6"		5'-6 1/2" AVG. 4'-3" TO 6'-10"
P31	#5	3	BENT	1'-0" TO 2'-9"	1'-6"		4'-10 1/2" AVG. 4'-0" TO 5'-9"
P32	#5	1	BENT	3"	4'-4"	1'-6"	7'-7"
P33	#5	1	BENT	5'-6"	1'-6"		8'-6"
P34	#5	4	BENT	2'-5" TO 5'-0"	1'-6"		6'-8 1/2" AVG. 5'-5" TO 8'-0"
S1	#5	76	BENT				11'-11"
ST1	#5	70	BENT	3'-2"	1'-6"		6'-2"
ST2	#5	10	BENT	6'-8"	1'-6"		9'-8"
ST3	#5	5	BENT	6'-10"	1'-6"		9'-10"
ST4	#5	15	BENT	8'-0"	1'-6"		11'-0"
ST5	#5	5	BENT	8'-4"	1'-6"		11'-4"
REINFORCING STEEL							
CS1	W20	1	SPIRAL				265'-9"



P4, P8, P12, P16 #5 (Pier No. 1)
P20, P24, P28, P32 #5 (Pier No. 2)



1-44 OVER I-244 NB BRIDGE "A"		TULSA COUNTY		DESIGN	JGS	5/16
				DETAIL	JGS	5/16
				CHECK	BRT	5/16
				GARVER		
STATE OF OKLAHOMA	DEPARTMENT OF TRANSPORTATION					
	JOB PIECE NO. 29775(04)	SHEET NO. 25				