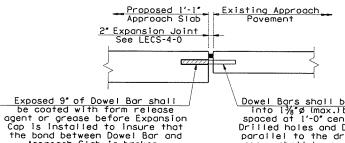
	BAR LIST - APPROACH SLAB NO. 1								
	MARK	SIZE	NO.	FORM	LENGTH	LENGTH VARIATION			
	EPOXY COATED REINFORCING STEEL								
I CONSTRUCTION	AL I	#4	45	STR.	28'-5"	-			
	AL2	#4	2	STR.	27'-9" AVG.	27'-8" TO 27'-10"			
	AL3	#4	1	STR.	14'-2"	-			
	AL4	#4	1	STR.	27'-8"	-			
	ATI	#4	25	STR.	27'-5"	-			
	AT2	#4	24	STR.	27'-31/2" AVG.	26'-3" TO 28'-4"			
	AW1	#5	1	BENT	27'-11"	*			
	AW2	#5	1	BENT	25'-10 <b>"</b>				
	BL I	#9	65	STR.	28'-5"	-			
	BL2	#9	2	STR.	27'-111/2" AVG.	27'-9" TO 28'-2"			
	BL3	#9	1	STR.	27'-8"	-			
	BL4	#9	1	STR.	22'-2"	-			
	BL5	#9	1	STR.	12'-0"	-			
ш	BL6	#9	1	STR.	27'-8"	•			
PHASE	BT1	#4	24	STR.	57'-11/2" AVG.	56'-1" TO 58'-2"			
	BWI	#5	1	BENT	55'-11"	-			
	BW2	#5	1	BENT	55'-10"	•			
	C 1	#4	24	STR.	2'-0"	-			
	D1	#5	2	STR.	3'-0"	<u>.</u>			
	EP1	#5	28	BENT	5'-4"	-			
T 1 0N	ALI	#4	40	STR.	28'-5"	-			
	AL5	#4	3	STR.	28'-2"	-			
	AT3	#4	25	STR.	23'-6"	-			
2	AT4	#4	23	STR.	24'-61/2" AVG.	24'-6" TO 24'-7"			
$\mathbb{Z}$	AT5	#4	1	STR.	23'-9"	*			
5	AW3	#5	1	BENT	24'-0"	-			
CONSTRUC	AW4	#5	1	BENT	24'-1"	-			
	BL1	#9	58	STR.	28'-5"	-			
_	BL7	#9	4	STR.	28'-2"	-			
_	вта	#4	23	STR.	50'-5 <b>"</b>	-			
	втз	#4	1	STR.	49'-6"	-			
S	BW3	#5	1	BENT	48'-0"	-			
PHASE	BW4	#5	1	BENT	48'-2"	-			
م	EP1	#5	32	BENT	5'-4"	-			

	BAR LIST - APPROACH SLAB NO. 2							
	MARK	SIZE	NO.	FORM	LENGTH	LENGTH VARIATION		
EPOXY COATED REINFORCING STEEL								
CONSTRUCTION	ALI	#4	31	STR.	28'-5"	-		
	AL2	#4	2	STR.	27'-101/2" AVG.	27'-7" TO 28'-2"		
	AL3	#4	1	STR.	24'-5"			
	AL4	#4	1	STR.	9'-1"	-		
	AL5	#4	1	STR.	27'-5"	-		
	ATI	#4	25	STR.	19'-0"	-		
	AT2	#4	23	STR.	20'-2" AVG.	19'-2" TO 21'-2"		
	AT3	#4	1	STR.	19'-0"	-		
	AW1	#5	1	BENT	20'-10"	-		
	AW2	#5	1	BENT	18'-8"	-		
	BL1	#9	45	STR.	28'-5"	-		
	BL2	#9	4	STR.	27'-10" AVG.	28'-5" TO 27'-3"		
	BL3	#9	ı	STR.	20'-7"	-		
_	BL4	#9	1	STR.	10'-6"	-		
ш	BL5	#10	1	STR.	27'-1"	-		
PHASE	BT1	#4	24	STR.	40'-5" AVG.	38'-4" TO 42'-6"		
	BWI	#5	1	BENT	40'-5"	-		
	BW2	#5	1	BENT	38'-3"	-		
	C1	#4	24	STR.	2'-0"	•		
	Dl	#5	2	STR.	3'-0"	-		
	EPI	#5	27	BENT	5'-4"	-		
z	ALI	#4	39	STR.	28'-5"			
0	AL6	#4	1	STR.	28'-7"	-		
-	AL7	#4	2	STR.	28'-10"	*		
CONSTRUCTION	AT4	#4	25	STR.	23'-6"	•		
<u>~</u>	AT5	#4	24	STR.	24'-2"	-		
S	AW3	#5	l	BENT	23'-9"	**		
Z	AW4	#5	1	BENT	23'-8"	-		
S	BL1	#9	57	STR.	28'-5"			
_	BL6	#9	1	STR.	28'-7"	-		
-	BL7	#9	3	STR.	28'-10"	-		
اس	BT2	#4	24	STR.	50'-0"	-		
PHASE	BW3	#5	1	BENT	47'-10"	•		
	BW4	#5	1	BENT	47'-8"	-		
۵	EP1	#5	28	BENT	5'-4"	-		



<u>NOTES:</u>

All costs of installation of Terminal Joint, including dowel bars, epoxy, expansion joint, materials, lobor, equipment, and any incidentals necessary to complete the work as shown shall be included in "APPROACH SLAB".

 OKLAHOMA DEPARTMENT OF TRANSPORTATION

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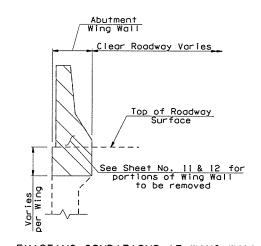
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 DESCRIPTION
 REVISIONS
 DATE

For details of dowel bars, see Std. CRCP2-3-0.

Dowel Bars shall be epoxied (non-capped end) into 1% of (max.) by 9 deep drilled holes, spaced at 1'-0' centers, placed at mid-slab. Drilled holes and Dowel Bars shall be placed parallel to the driving surface. Sufficient epoxy shall be used to completely fill the void between the Dowel Bar and the hole.

DETAIL "B"

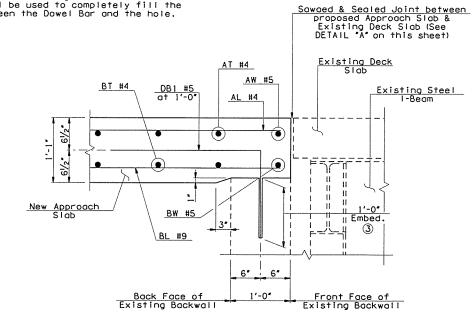


Approach Slab is broken.

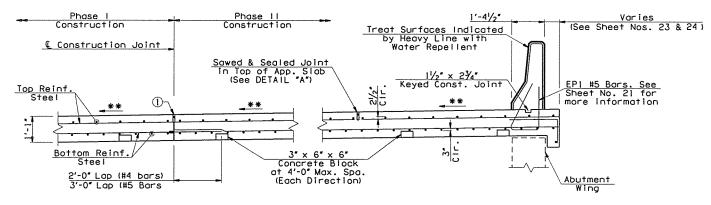
EXISTING CONDITIONS AT WING WALL

Rapid Cure Joint Sealant

Grind to 1/4" Chamfer (Typ.)



## SECTION C-C



Approach Slab or Deck Slab DETAIL "A"

1/2" MOX

② This dimension shall taper from  $\frac{1}{2}$  at edge of driving lane/shoulder to  $\frac{1}{8}$  at rail for Transverse Joints only.

SECTION A THRU APPROACH SLAB

SECTION B

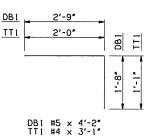
- ① ½" Longitudinal Sawed and Sealed Construction Joint in the top of Approach Slab. See DETAIL "A".
- \*\* Match cross slope currently on existing Approach Slobs.

## NOTES:

Do not groove within 6" of any joint.

Place reinforcing steel in the top of the Approach Slab 2" from either side of the Sawed & Sealed Longitudinal Joints.

See Sheet No. 21 for parapet reinforcing.

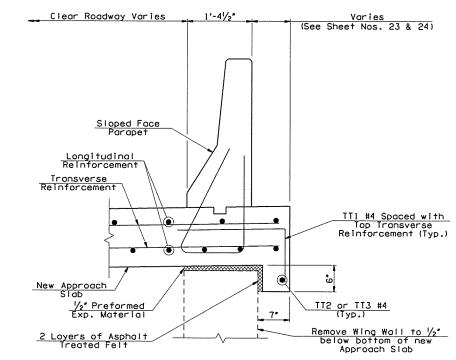


3 ANCHORAGE SYSTEM:

The Contractor shall use an Anchorage System that has been approved by ODOT's materials division. The Anchorage System shall be capable of developing the full strength of the reinforcing steel that is to be anchored. The embedment depth shown is to be adjusted to meet the Manufacturer's requirements. Anchorages shall be installed in accordance with the Manufacturer's specifications for the system used.

Drilling into the existing concrete to install the anchorage shall be accomplished without cutting existing concrete reinforcing steel bars. Prior to drilling, the Contractor shall locate and mark the existing concrete reinforcing steel bars with non-destructive tools, equipment and methods approved by the Engineer. If existing reinforcing steel bars are encountered during drilling, the drilling shall cease and the hole shall be grouted. The hole shall then be relocated to clear the existing reinforcing steel bars. Any adjustment in the locations of the new DBI Bars from the plan locations shown shall be the minimum amount necessary to avoid cutting the existing concrete reinforcing steel bars and shall be approved by the Engineer.

All costs of the Anchorage Assemblies including labor, materials, tools, drilling, and incidentals necessary to complete the work shown in the plans shall be included in the price bid per Square Yard of "APPROACH SLAB".



NEW APPROACH SLAB AT WING WALL

I-44 WB OVER S 38TH W AVE TULSA COUNTY DESIGN JMO 9/15 & TSU RR BRIDGE 'A' DETAIL NBK 11/15 CHECK BRT 11/15 GARVER

STATE OF DEPARTMENT OF TRANSPORTATION OKLAHOMA JOB PIECE NO. 28872(04) SHEET NO. 25

9:26:28 AM K-\2013\13037393 - ODOT FC 1483R SRR Task Order 13\Drawinas\RRIDGF\RRIDGF 4\in28872(04)-a apps3 day

THEA COUNTY