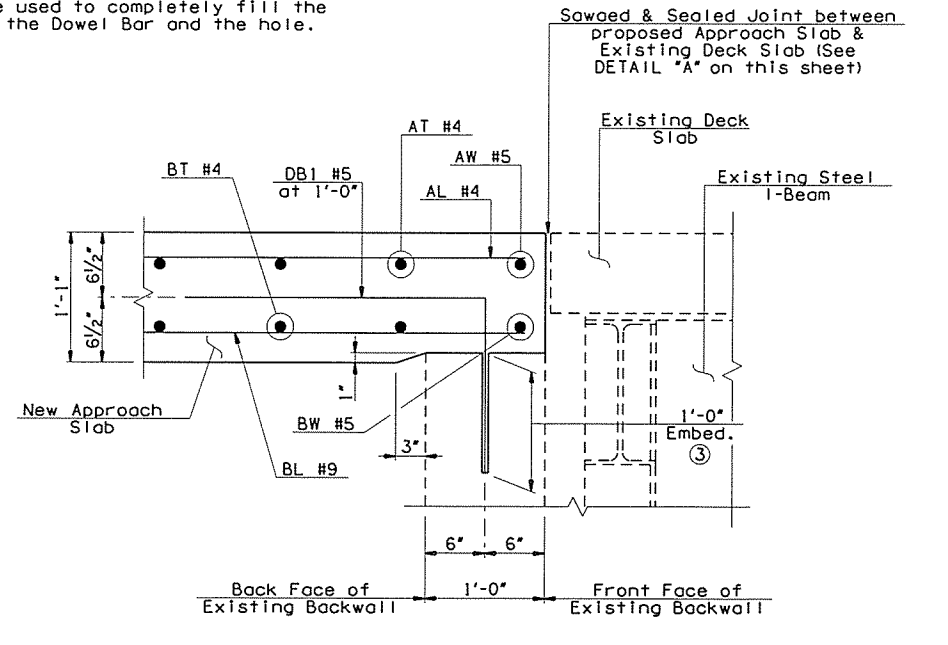
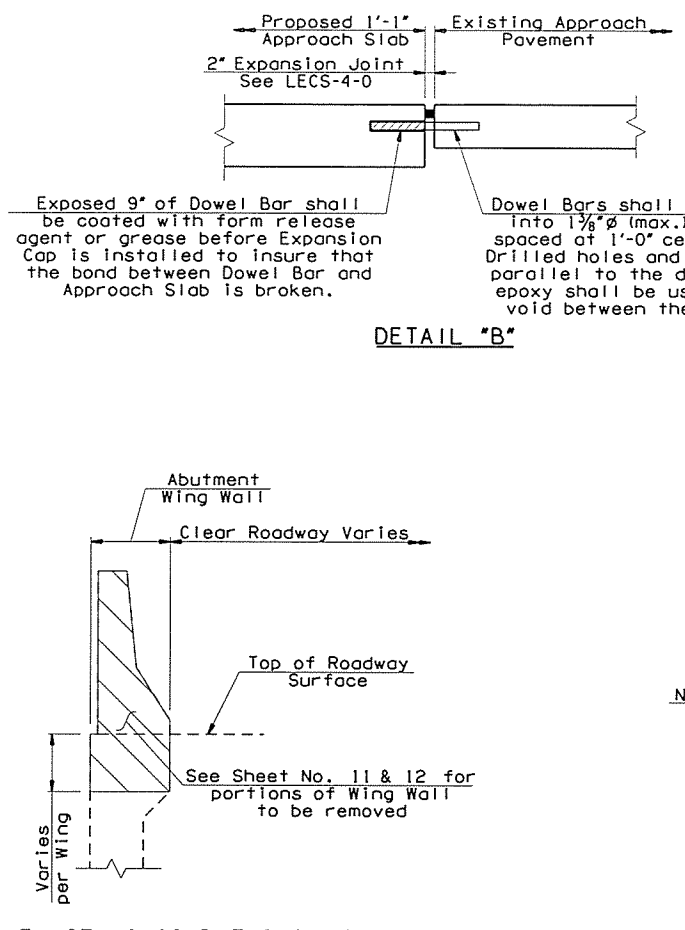


NOTES:
 All costs of installation of Terminal Joint, including dowel bars, epoxy, expansion joint, materials, labor, equipment, and any incidentals necessary to complete the work as shown shall be included in "APPROACH SLAB".
 For details of dowel bars, see Std. CRCP2-3-0.

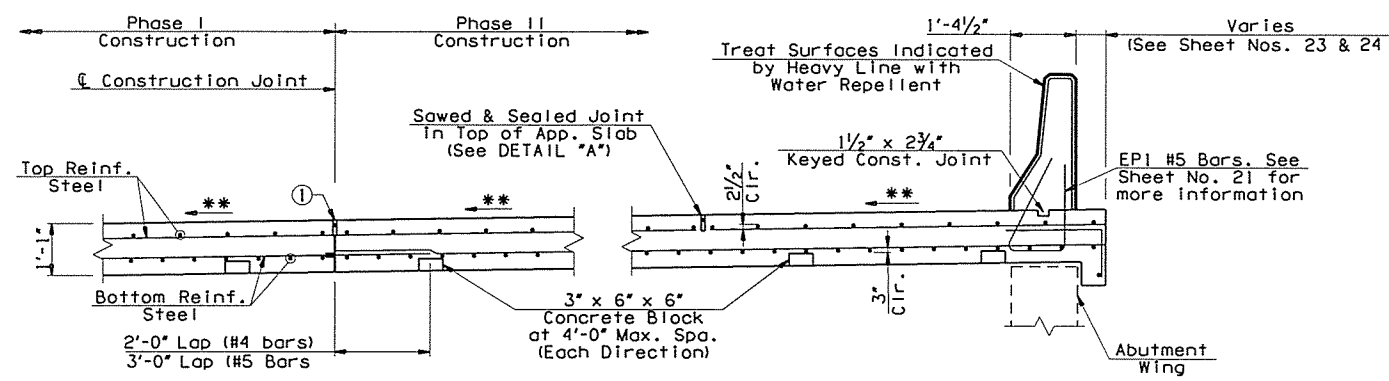
BAR LIST - APPROACH SLAB NO. 1					
MARK	SIZE	NO.	FORM	LENGTH	LENGTH VARIATION
EPOXY COATED REINFORCING STEEL					
AL1	#4	45	STR.	28'-5"	-
AL2	#4	2	STR.	27'-9" AVG.	27'-8" TO 27'-10"
AL3	#4	1	STR.	14'-2"	-
AL4	#4	1	STR.	27'-8"	-
AT1	#4	25	STR.	27'-5"	-
AT2	#4	24	STR.	27'-3 1/2" AVG.	26'-3" TO 28'-4"
AW1	#5	1	BENT	27'-11"	-
AW2	#5	1	BENT	25'-10"	-
BL1	#9	65	STR.	28'-5"	-
BL2	#9	2	STR.	27'-11 1/2" AVG.	27'-9" TO 28'-2"
BL3	#9	1	STR.	27'-8"	-
BL4	#9	1	STR.	22'-2"	-
BL5	#9	1	STR.	12'-0"	-
BL6	#9	1	STR.	27'-8"	-
BT1	#4	24	STR.	57'-1 1/2" AVG.	56'-1" TO 58'-2"
BW1	#5	1	BENT	55'-11"	-
BW2	#5	1	BENT	55'-10"	-
C1	#4	24	STR.	2'-0"	-
D1	#5	2	STR.	3'-0"	-
EP1	#5	28	BENT	5'-4"	-
PHASE I CONSTRUCTION					
AL1	#4	40	STR.	28'-5"	-
AL5	#4	3	STR.	28'-2"	-
AT3	#4	25	STR.	23'-6"	-
AT4	#4	23	STR.	24'-6 1/2" AVG.	24'-6" TO 24'-7"
AT5	#4	1	STR.	23'-9"	-
AW3	#5	1	BENT	24'-0"	-
AW4	#5	1	BENT	24'-1"	-
BL1	#9	58	STR.	28'-5"	-
BL7	#9	4	STR.	28'-2"	-
BT2	#4	23	STR.	50'-5"	-
BT3	#4	1	STR.	49'-6"	-
BW3	#5	1	BENT	48'-0"	-
BW4	#5	1	BENT	48'-2"	-
EP1	#5	32	BENT	5'-4"	-

BAR LIST - APPROACH SLAB NO. 2					
MARK	SIZE	NO.	FORM	LENGTH	LENGTH VARIATION
EPOXY COATED REINFORCING STEEL					
AL1	#4	31	STR.	28'-5"	-
AL2	#4	2	STR.	27'-10 1/2" AVG.	27'-7" TO 28'-2"
AL3	#4	1	STR.	24'-5"	-
AL4	#4	1	STR.	9'-1"	-
AL5	#4	1	STR.	27'-5"	-
AT1	#4	25	STR.	19'-0"	-
AT2	#4	23	STR.	20'-2" AVG.	19'-2" TO 21'-2"
AT3	#4	1	STR.	19'-0"	-
AW1	#5	1	BENT	20'-10"	-
AW2	#5	1	BENT	18'-8"	-
BL1	#9	45	STR.	28'-5"	-
BL2	#9	4	STR.	27'-10" AVG.	28'-5" TO 27'-3"
BL3	#9	1	STR.	20'-7"	-
BL4	#9	1	STR.	10'-6"	-
BL5	#10	1	STR.	27'-1"	-
BT1	#4	24	STR.	40'-5" AVG.	38'-4" TO 42'-6"
BW1	#5	1	BENT	40'-5"	-
BW2	#5	1	BENT	38'-3"	-
C1	#4	24	STR.	2'-0"	-
D1	#5	2	STR.	3'-0"	-
EP1	#5	27	BENT	5'-4"	-
PHASE I CONSTRUCTION					
AL1	#4	39	STR.	28'-5"	-
AL6	#4	1	STR.	28'-7"	-
AL7	#4	2	STR.	28'-10"	-
AT4	#4	25	STR.	23'-6"	-
AT5	#4	24	STR.	24'-2"	-
AW3	#5	1	BENT	23'-9"	-
AW4	#5	1	BENT	23'-8"	-
BL1	#9	57	STR.	28'-5"	-
BL6	#9	1	STR.	28'-7"	-
BL7	#9	3	STR.	28'-10"	-
BT2	#4	24	STR.	50'-0"	-
BW3	#5	1	BENT	47'-10"	-
BW4	#5	1	BENT	47'-8"	-
EP1	#5	28	BENT	5'-4"	-



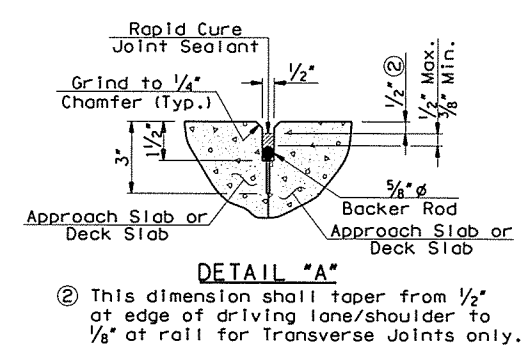
EXISTING CONDITIONS AT WING WALL

SECTION C-C

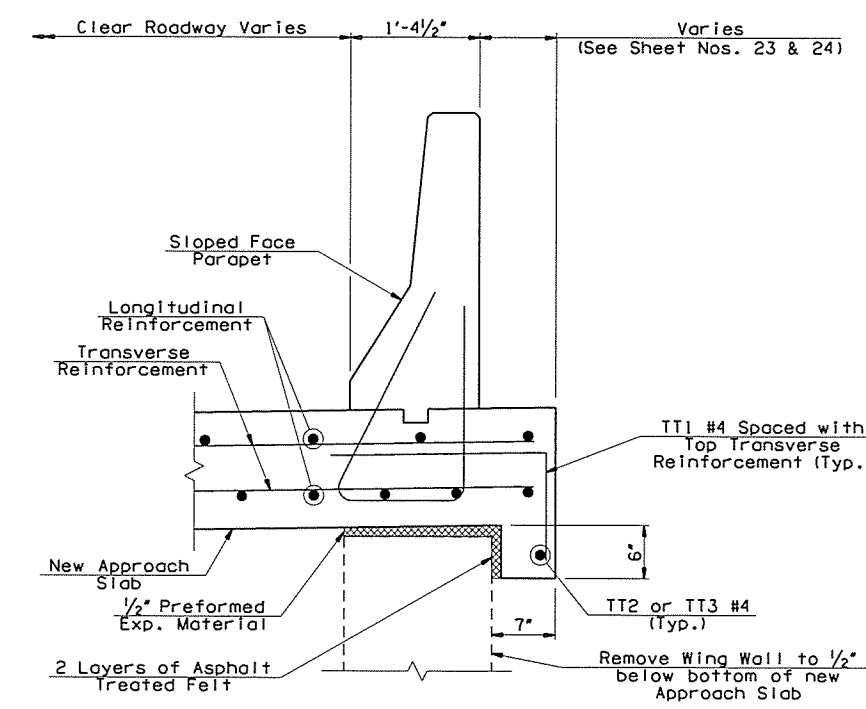


SECTION A THRU APPROACH SLAB

SECTION B

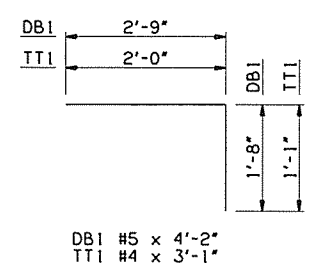


DETAIL 'A'



NEW APPROACH SLAB AT WING WALL

① 1/2" Longitudinal Sawed and Sealed Construction Joint in the top of Approach Slab. See DETAIL "A".
 ** Match cross slope currently on existing Approach Slabs.



③ ANCHORAGE SYSTEM:
 The Contractor shall use an Anchorage System that has been approved by ODOT's materials division. The Anchorage System shall be capable of developing the full strength of the reinforcing steel that is to be anchored. The embedment depth shown is to be adjusted to meet the Manufacturer's requirements. Anchorages shall be installed in accordance with the Manufacturer's specifications for the system used.
 Drilling into the existing concrete to install the anchorage shall be accomplished without cutting existing concrete reinforcing steel bars. Prior to drilling, the Contractor shall locate and mark the existing concrete reinforcing steel bars with non-destructive tools, equipment and methods approved by the Engineer. If existing reinforcing steel bars are encountered during drilling, the drilling shall cease and the hole shall be grouted. The hole shall then be relocated to clear the existing reinforcing steel bars. Any adjustment in the locations of the new DB1 Bars from the plan locations shown shall be the minimum amount necessary to avoid cutting the existing concrete reinforcing steel bars and shall be approved by the Engineer.
 All costs of the Anchorage Assemblies including labor, materials, tools, drilling, and incidentals necessary to complete the work shown in the plans shall be included in the price bid per Square Yard of "APPROACH SLAB".

NOTES:
 Do not groove within 6" of any joint.
 Place reinforcing steel in the top of the Approach Slab 2" from either side of the Sawed & Sealed Longitudinal Joints.
 See Sheet No. 21 for parapet reinforcing.