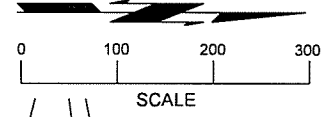


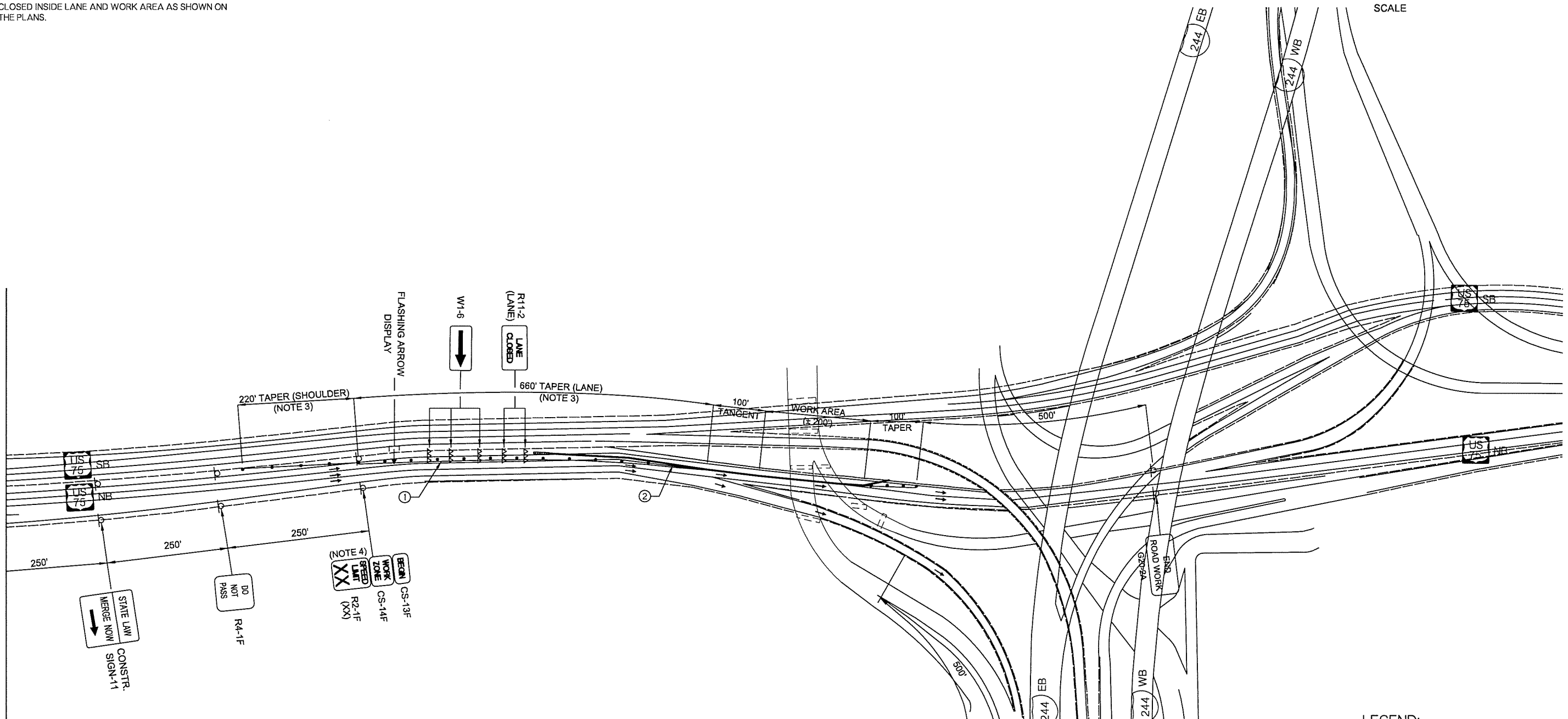
DESCRIPTION	REVISIONS	DATE



**TRAFFIC CONTROL KEY NOTES**

- ① CLOSE NB I-75 INSIDE LANE TO PROVIDE ADEQUATE WORK AREA FOR PIER 2 AND PIER 3 RECONSTRUCTION.
- ② PLACE PORTABLE LONGITUDINAL BARRIER ALONG CLOSED INSIDE LANE AND WORK AREA AS SHOWN ON THE PLANS.

MATCH LINE - NB US-75 INSIDE LANE CLOSURE - SHEET 2 OF 3



REMOVE ANY CONFLICTING PAVEMENT MARKING

- LEGEND:**
- SIGN
  - CHANNELIZING DEVICE (DRUM)
  - ARROW DISPLAY
  - ATTENUATOR
  - TYPE III BARRICADE
  - PORT. LONGITUDINAL BARRIER
  - BARRIER

**NOTE 1:** MAXIMUM SPACING BETWEEN CHANNELIZING DEVICES (FEET) SHALL BE EQUAL TO THE POSTED SPEED LIMIT (M.P.H.) WITH THE FOLLOWING EXCEPTIONS. SPACING SHALL NOT EXCEED 25 FEET FOR CONES OR TUBE CHANNELIZERS. SPACING SHALL NOT EXCEED 50 FEET FOR TYPE II BARRICADES, VERTICAL PANELS OR DRUMS.

**NOTE 2:** MAXIMUM SPACING BETWEEN CHANNELIZING DEVICES (FEET) SHALL BE EQUAL TO TWICE THE POSTED SPEED LIMIT (M.P.H.) WITH THE FOLLOWING EXCEPTIONS. SPACING SHALL NOT EXCEED 50 FEET FOR CONES OR TUBE CHANNELIZERS. SPACING SHALL NOT EXCEED 100 FEET FOR TYPE II BARRICADES, VERTICAL PANELS OR DRUMS.

**NOTE 3:** A MINIMUM OF FIVE (5) CHANNELIZING DEVICES SHALL BE PLACED THROUGH THIS TAPER.

**NOTE 4:** CONSTRUCTION SPEED LIMIT TO BE DETERMINED BY THE DIVISION ENGINEER.

**NOTE 5:** DOWNSTREAM TAPERS SHALL CONTAIN A MINIMUM OF FOUR (4) CHANNELIZING DEVICES.

**NOTE 6:** SEE "SUMMARY OF PAY QUANTITIES (TRAFFIC)" FOR DETAIL

FOR ADDITIONAL INFORMATION ABOUT TAPER LENGTHS AND SPACING OF CHANNELIZING DEVICES, SEE STANDARD DRAWING TCS2-1-(LATEST REVISIONS).

DESIGN		<b>OKLAHOMA DEPARTMENT OF TRANSPORTATION</b>  TRAFFIC CONTROL PLAN NB US-75 INSIDE LANE CLOSURE SHEET 3 OF 3 STATE JOB NO. 28865(04) SHEET NO. 86 TULSA CO. 2ND STREET
DRAWN		
CHECKED		
APPROVED		
CREW		