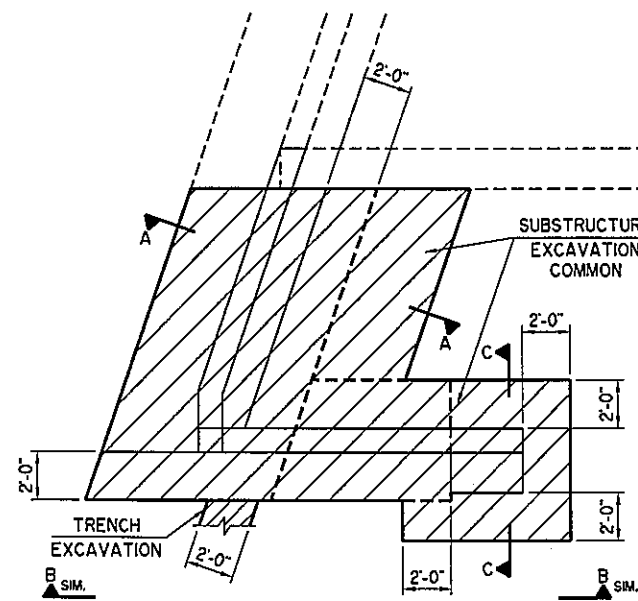
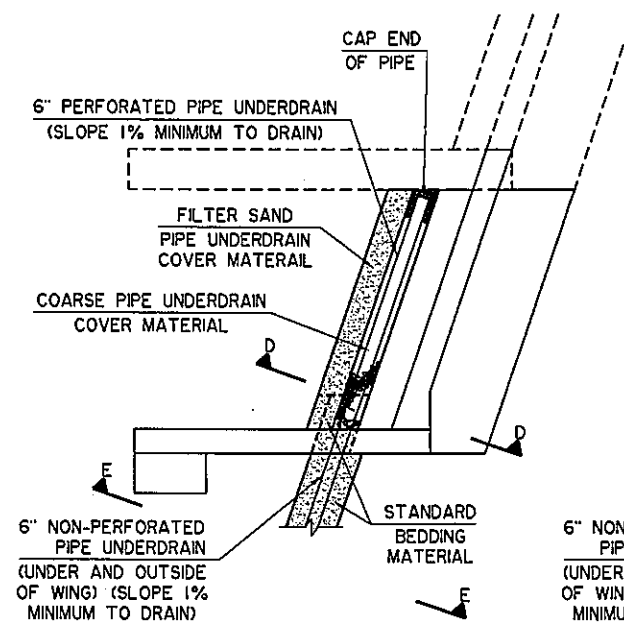


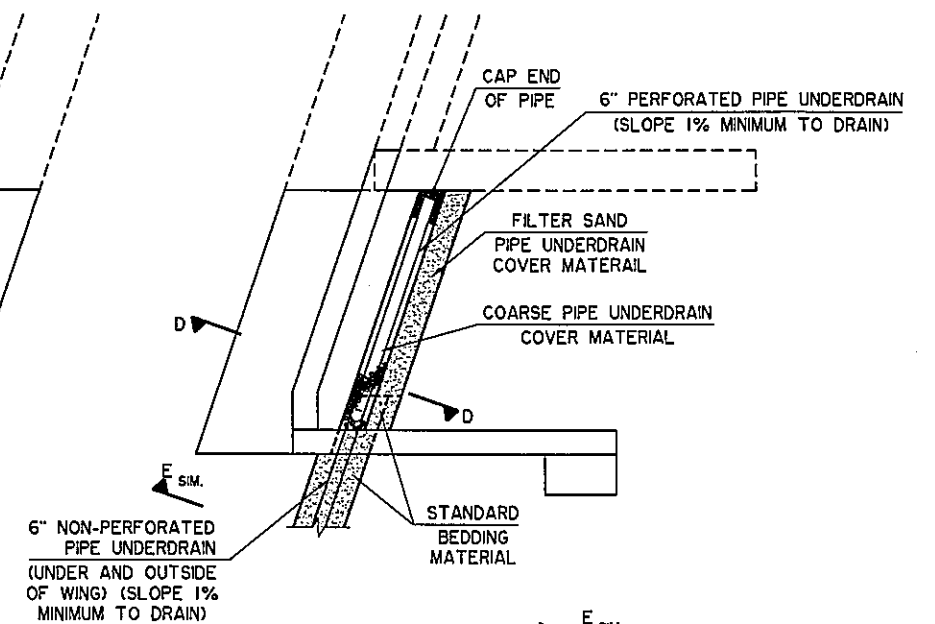
**EXCAVATION PLAN**  
ABUTMENT NO. 1



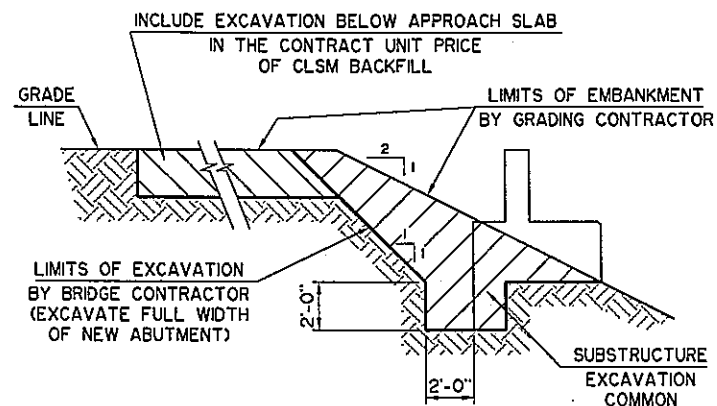
**EXCAVATION PLAN**  
ABUTMENT NO. 2



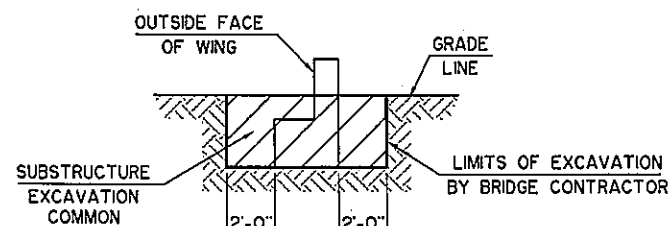
**PIPE UNDERDRAIN PLAN**  
ABUTMENT NO. 1



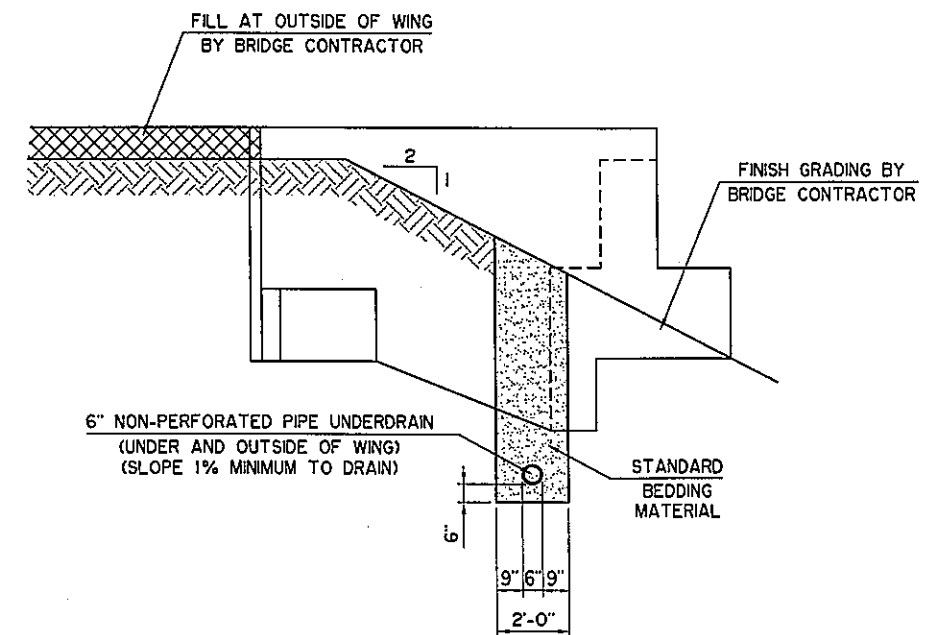
**PIPE UNDERDRAIN PLAN**  
ABUTMENT NO. 2



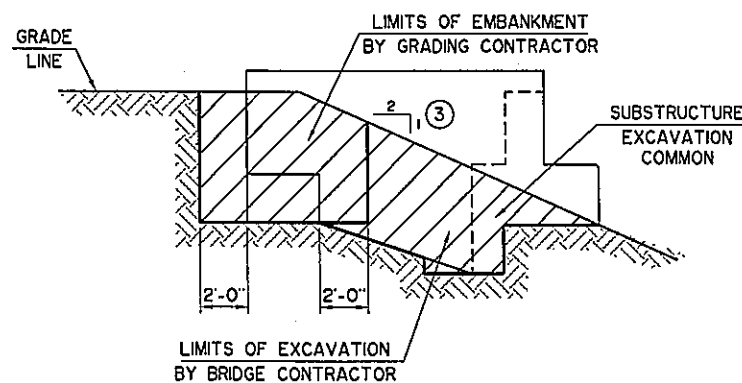
**SECTION A-A**



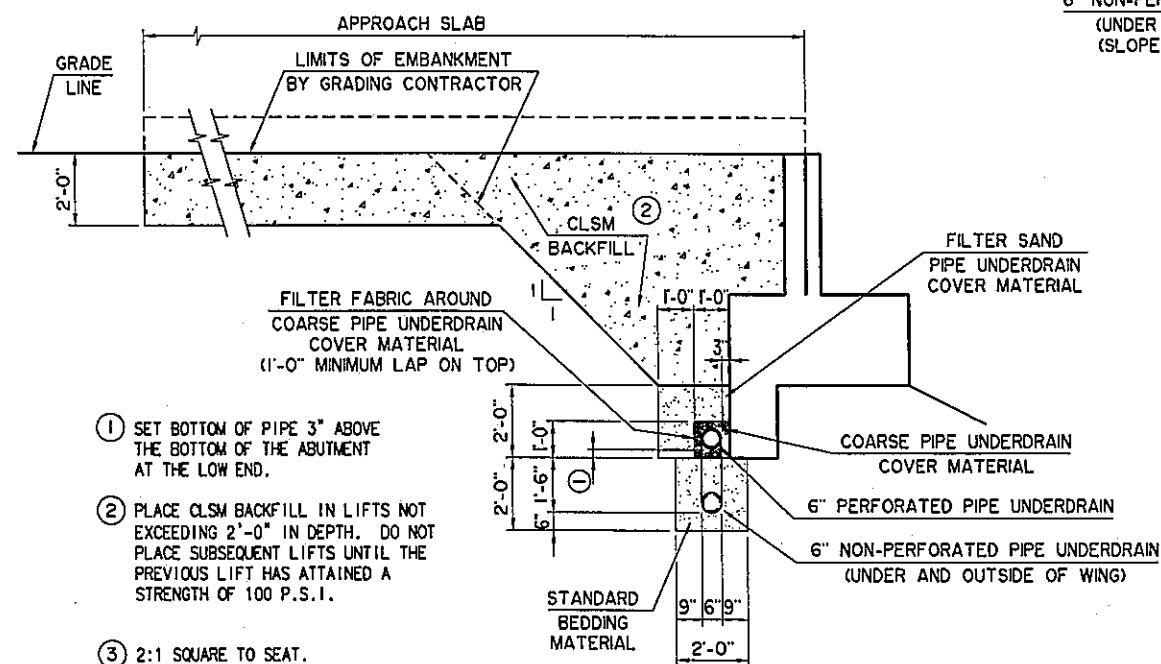
**SECTION C-C**



**SECTION E-E**



**SECTION B-B**



**SECTION D-D**

- ① SET BOTTOM OF PIPE 3" ABOVE THE BOTTOM OF THE ABUTMENT AT THE LOW END.
- ② PLACE CLSM BACKFILL IN LIFTS NOT EXCEEDING 2'-0" IN DEPTH. DO NOT PLACE SUBSEQUENT LIFTS UNTIL THE PREVIOUS LIFT HAS ATTAINED A STRENGTH OF 100 P.S.I.
- ③ 2:1 SQUARE TO SEAT.

DO NOT PLACE CLSM BACKFILL UNTIL THE SUPERSTRUCTURE IS IN PLACE AND THE ABUTMENT WING CONCRETE HAS ATTAINED A STRENGTH OF 3000 P.S.I.

Design	TAC		<b>U.S. HIGHWAY 69 - McALESTER</b> <b>SUBSTRUCTURE EXCAVATION AND</b> <b>PIPE UNDERDRAIN DETAILS</b> AOK RAILROAD BRIDGE "S" NORTHBOUND
Drawn	KGL	HEJ	
Checked	ADT	KGL	
Approved	CEG		
Squad	WEA		
JOB PIECE NO. 14999(04)			SHEET NO. B31