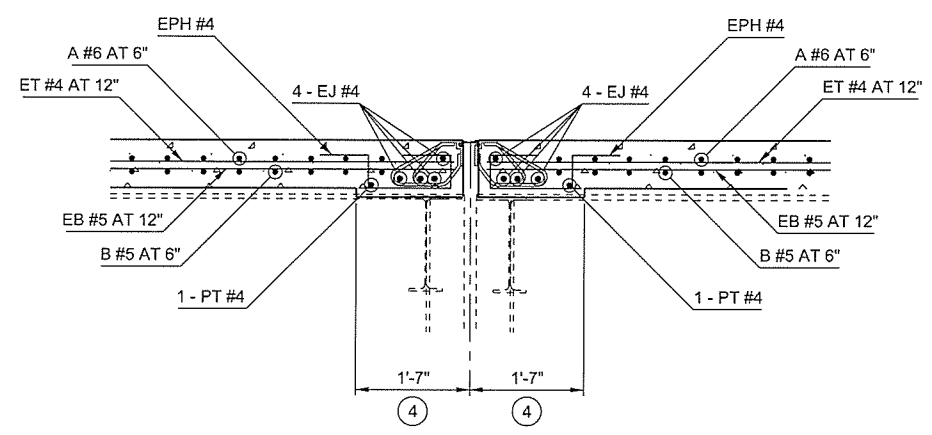
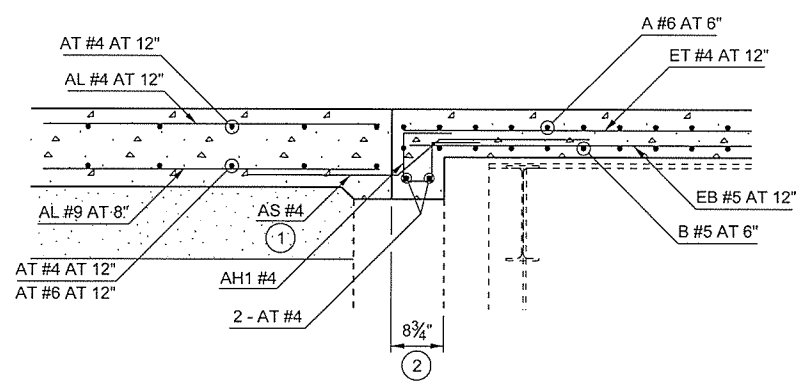


SECTION A-A

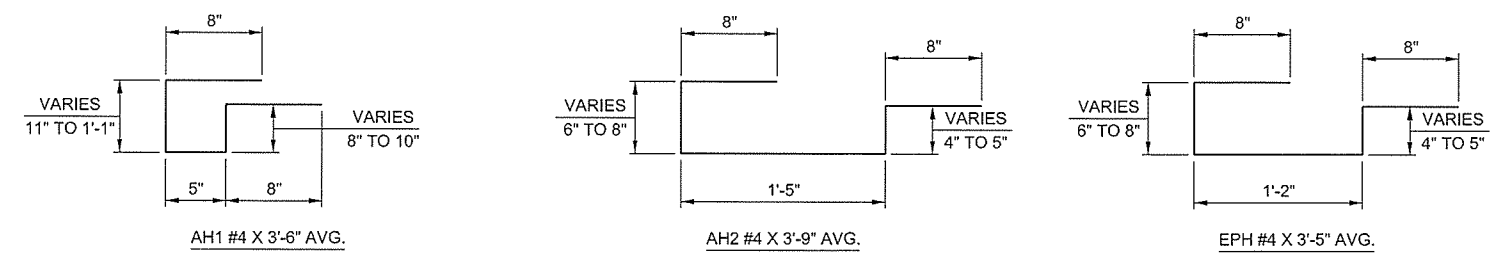
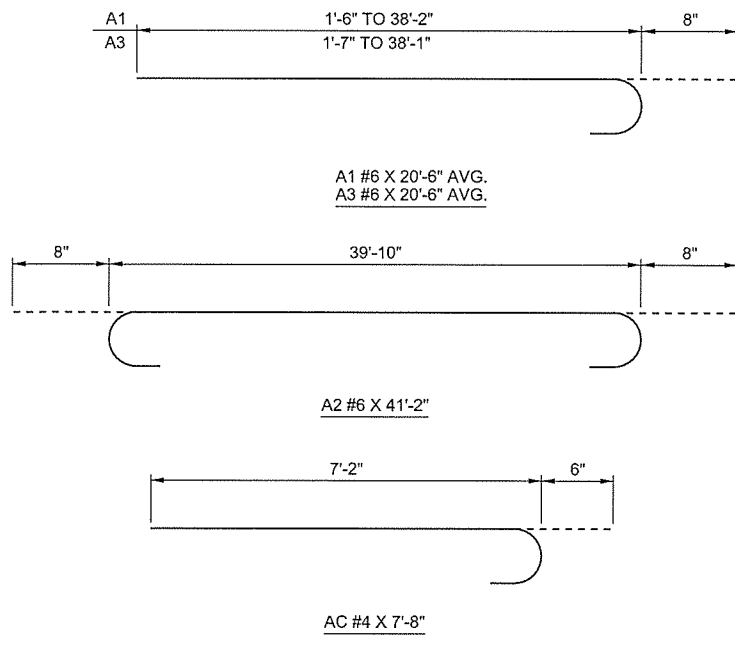
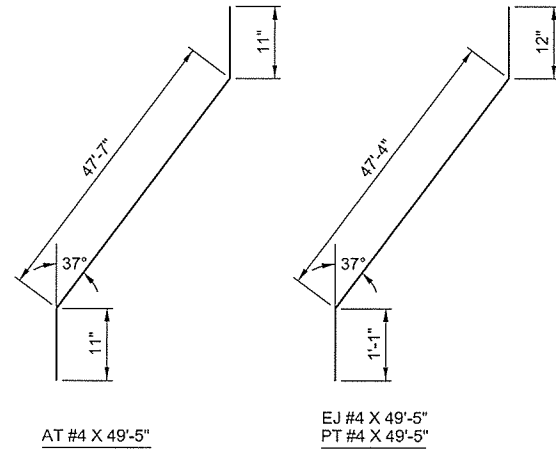
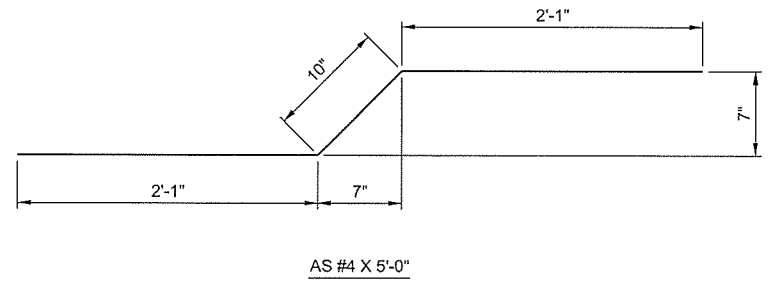
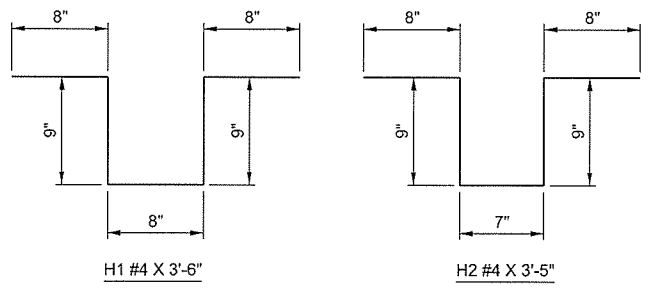


SECTION C-C



SECTION B-B

- ① AS BARS SHALL BE TIED TO THE TOP LAYER OF REINFORCING STEEL IN THE DECK SLAB AND TO THE BOTTOM LAYER REINFORCING STEEL IN THE APPROACH SLAB.
- ② DIMENSION IS 7" PERPEDICULAR TO FR. FC. OF BACKWALL
- ③ DIMENSION IS 1'-5 3/4" PERPEDICULAR TO FR. FC. OF BACKWALL
- ④ DIMENSION IS 1'-3 3/16" PERPEDICULAR TO C OF PIER



SUPERSTRUCTURE BAR LIST - BRIDGE "A"						
BRIDGE "B" IDENTICAL						
(EPOXY COATED REINFORCED BARS)						
MARK	SIZE	FORM	NO.	LENGTH	LENTGH VARIATION	
[1]	A1	#6	BNT.	112	20'-6" AVG.	2'-2" TO 38'-10"
	A2	#6	BNT.	291	41'-2"	
[2]	A3	#6	BNT.	224	20'-6" AVG.	2'-3" TO 38'-9"
	AC	#4	BNT.	864	7'-8"	
[3]	AH1	#4	BNT.	82	3'-6" AVG.	3'-4" TO 3'-8"
[4]	AH2	#4	BNT.	72	3'-9" AVG.	3'-7" TO 3'-10"
	AS	#4	BNT.	82	5'-0"	
	AT	#4	BNT.	4	49'-5"	
[1]	B1	#5	STR.	112	19'-10" AVG.	1'-6" TO 38'-2"
	B2	#5	STR.	291	39'-10"	
[2]	B3	#5	STR.	224	19'-10" AVG.	1'-7" TO 38'-1"
	EB1	#5	STR.	39	45'-10"	
	EB2	#5	STR.	2	46'-0"	
[6]	EB3	#5	STR.	4	149'-8"	
[6]	EB4	#5	STR.	37	149'-7"	
	EB5	#5	STR.	2	39'-0"	
	EB6	#5	STR.	39	38'-10"	
	EPB	#4	STR.	40	44'-4"	
	EJ	#4	BNT.	16	49'-5"	
[5]	EPH	#4	BNT.	164	3'-5" AVG.	3'-4" TO 3'-7"
	EPT	#6	STR.	40	44'-4"	
	ET1	#4	STR.	39	45'-10"	
	ET2	#4	STR.	2	46'-0"	
[7]	ET3	#4	STR.	4	147'-10"	
[7]	ET4	#4	STR.	37	147'-9"	
	ET5	#4	STR.	2	39'-0"	
	ET6	#4	STR.	39	38'-10"	
	H1	#4	BNT.	292	3'-6"	
	H2	#4	BNT.	164	3'-5"	
	PT	#4	BNT.	4	49'-5"	
[8]	SR1	#5	BNT.	1,388	4'-1"	

- [1] INCLUDES 2 SETS OF 56 BARS
- [2] INCLUDES 4 SETS OF 56 BARS
- [3] INCLUDES 2 SETS OF 41 BARS
- [4] INCLUDES 2 SETS OF 36 BARS
- [5] INCLUDES 4 SETS OF 41 BARS
- [6] INCLUDES 2 - 2'-7" MINIMUM LAP
- [7] INCLUDES 2 - 1'-8" MINIMUM LAP
- [8] FOR BAR BEND, SEE STD. TR4-2

EXPANSION JOINT SETTING SCHEDULE - BRIDGE "A"		
BRIDGE "B" IDENTICAL		
EXP. JOINT OPENING	TEMPERATURE °F	
	PIER NO. 1	PIER NO. 3
15/8"	111	115
13/4"	94	96
17/8"	77	78
2"	60	60
2 1/8"	43	42
2 1/4"	26	24
2 3/8"	9	5

S.H. 74 OVER I-35 McCLAIN CO.

DESIGN	GDD	OKLAHOMA DEPARTMENT OF TRANSPORTATION SLAB REINFORCING DETAILS STATE JOB NO. 29572(04) SHEET NO. 22
DRAWN	ZTF	
CHECKED	JTK	
APPROVED		
SQUAD	MacArthur	

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