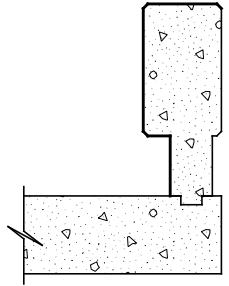


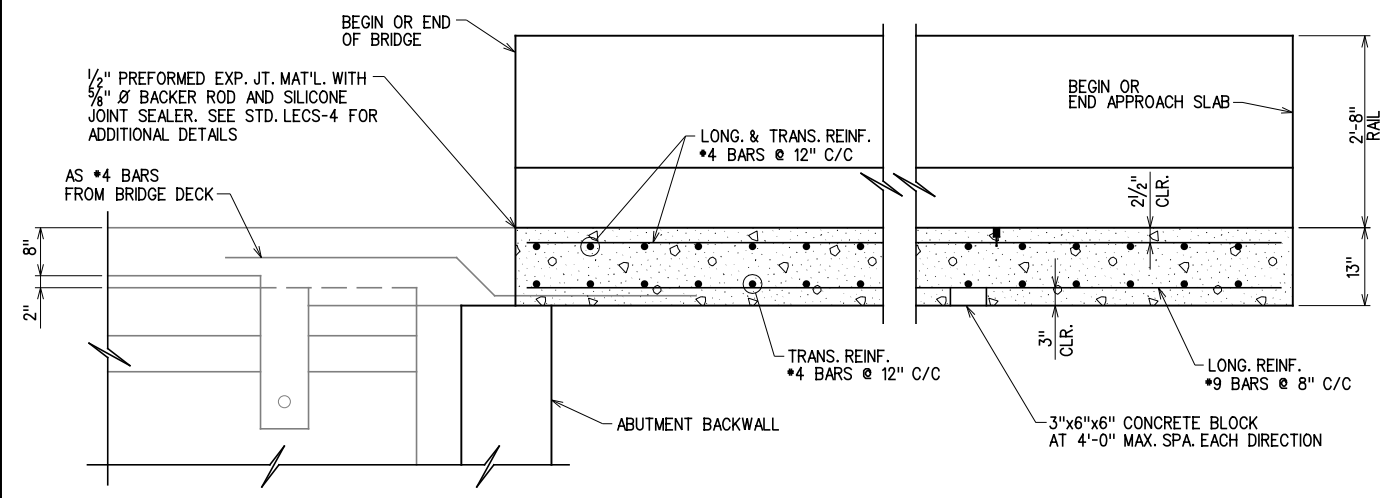
**TRANSVERSE SECTION THRU APPROACH SLAB**  
(SECTION LOOKING UP STATION)

NOTE:  
THE REINFORCING STEEL IN THE TOP OF THE APPROACH SLAB SHALL END 2" EITHER SIDE OF THE LONGITUDINAL JOINT

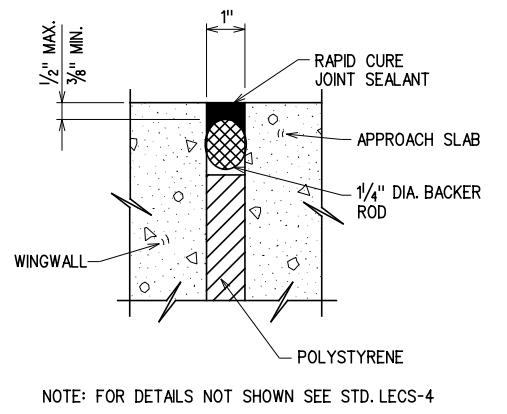


**PENETRATING WATER REPELLENT TREATMENT**

NOTE:  
SURFACES INDICATED WITH HEAVY LINES SHALL BE TREATED WITH A PENETRATING WATER REPELLENT SURFACE TREATMENT.

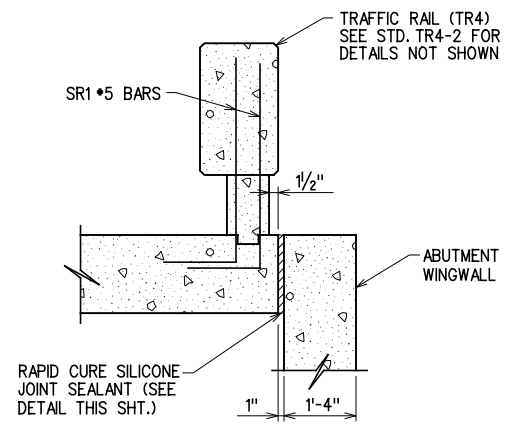


**LONGITUDINAL SECTION THRU APPROACH SLAB**

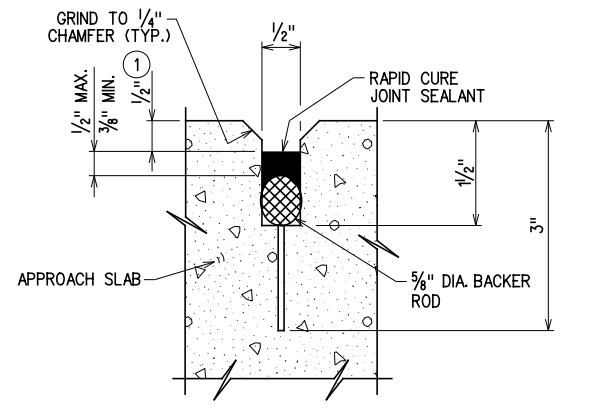


**DETAIL OF RAPID CURE JOINT SEALANT BETWEEN WING WALL AND APPROACH SLAB**

NOTE: FOR DETAILS NOT SHOWN SEE STD. LECS-4



**DETAIL OF APPROACH SLAB AT ABUTMENT WING**



**DETAIL "A"**

① THIS DIMENSION SHALL TAPER FROM 1/2" AT EDGE OF DRIVING LANE/SHOULDER TO 1/8" AT RAIL FOR TRANSVERSE JOINTS ONLY.

X:\guernsey\EC-1360\Struc\Bridge\_C\APPR.dgn 6/2/2016



**GROSSMAN & KEITH**  
ENGINEERING COMPANY  
10408 GREENBRIAR PL., OKLA. CITY OK. 73159  
PH. 691-3213 FAX 691-3214  
CA. #74 EXPIRES 06/30/2016

DESIGN	
DRAWN	
CHECKED	
APPROVED	
SQUAD	G/K ENGR.

U.S. 277 OVER DEEP RED CREEK OVERFLOW COTTON COUNTY  
BRIDGE "C"  
**APPROACH SLAB DETAILS**  
(2 OF 2)  
JOB PIECE NO. 28036(04) SHEET NO. 96