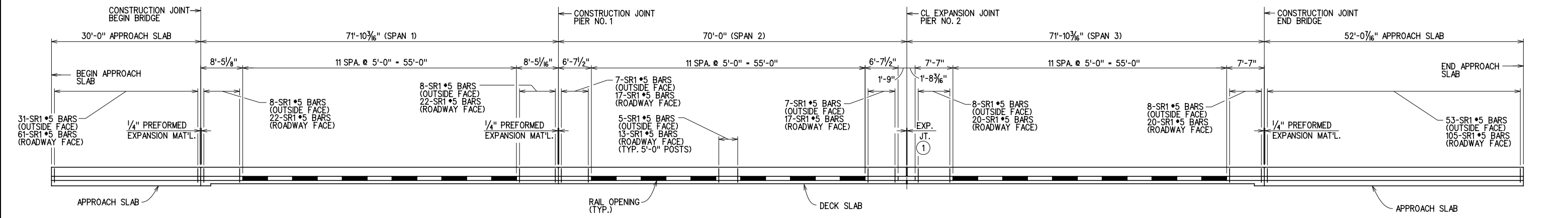


LONGITUDINAL SECTION



TRAFFIC RAIL LAYOUT  
(RT. SIDE SHOWN)

DECK SLAB NOTES

- EPOXY-COAT OR GALVANIZE STEEL ITEMS USED TO FACILITATE CONSTRUCTION, SUCH AS DECK FORM HANGERS, TY-BAR CLIPS, INSERT WELD ANCHORS, OR OTHER APPURTENANCES, THAT WILL REMAIN IN PLACE IN THE DECK SLAB. EPOXY-COAT IN ACCORDANCE WITH AASHTO M284 OR GALVANIZE IN ACCORDANCE WITH AASHTO M111.
- THE DECK SLAB SHALL BE POURED ONE SPAN AT A TIME. NO ADJACENT SPAN AT A FIXED PIER SHALL BE POURED UNTIL AT LEAST 48 HOURS AFTER ANY ADJACENT POUR HAS BEEN COMPLETED. CONSTRUCTION JOINTS AT THE FIXED PIERS SHALL NOT BE KEYS. IN THE EVENT OF AN EMERGENCY, POURING OF DECK SLAB MAY BE HALTED WITH A CONSTRUCTION JOINT MADE PERPENDICULAR TO THE DIRECTION OF TRAFFIC AS DIRECTED BY THE ENGINEER. LONGITUDINAL REINFORCING SHALL BE CONTINUOUS THRU ALL CONSTRUCTION JOINTS. NO HEAVY EQUIPMENT WILL BE PERMITTED ON THE FINISHED DECK SLAB WITHIN 5' OF ANY CONSTRUCTION JOINT UNTIL THE DECK SLAB IS IN PLACE ON BOTH SIDES OF THE RESPECTIVE JOINT AND AT LEAST 48 HOURS HAS ELAPSED SINCE CONCRETE PLACEMENT.
- ALL CONSTRUCTION JOINTS WITHIN THE DECK SLAB SHALL BE SEALED USING HIGH MOLECULAR WEIGHT METHACRYLATE IN ACCORDANCE WITH SECTION 523 OF THE SPECIFICATIONS. ALL COST OF THE HIGH MOLECULAR WEIGHT METHACRYLATE SEALER SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE OF "SEALER RESIN". ALL COST FOR EQUIPMENT AND LABOR FOR THE INSTALLATION OF HIGH MOLECULAR WEIGHT METHACRYLATE SEALER SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE OF "SEALER CRACK PREPARATION", "SEALER RESIN" AND "SEALER CRACK PREPARATION" FOR EMERGENCY CONSTRUCTION JOINTS WILL NOT BE MEASURED FOR PAYMENT BY THE DEPARTMENT.
- DO NOT PLACE THE CONCRETE FOR THE DECK SLAB OR APPLY OTHER MASSIVE LOADS TO THE BEAMS OR DIAPHRAGMS UNTIL THE CONCRETE IN THE DIAPHRAGMS HAS BEEN IN PLACE A MINIMUM OF 10 DAYS, OR AT THE DISCRETION OF THE ENGINEER. THIS TIME MAY BE SHORTENED IF THE CONCRETE HAS ATTAINED 80% OF THE SPECIFIED COMPRESSIVE STRENGTH.
- DO NOT CUT GROOVES WITHIN 6" OF ANY CONSTRUCTION JOINT.

① MATCH THE WIDTH OF THE OPENING BETWEEN THE ENDS OF THE TRAFFIC RAIL WITH THE OPENING OF THE EXPANSION JOINT. SEE EXPANSION DEVICE SETTING TABLE SHT. 87.

NOTE:  
1. SEE STANDARD TR4-2 FOR TRAFFIC RAIL DETAILS NOT SHOWN.

**GROSSMAN & KEITH**  
ENGINEERING COMPANY  
10408 GREENBRIAR PL., OKLA. CITY OK. 73159  
PH. 691-3213 FAX 691-3214  
CA. #74 EXPIRES 06/30/2016



DESIGN		U.S. 277 OVER DEEP RED CREEK OVERFLOW	COTTON COUNTY
DRAWN		BRIDGE "C"	
CHECKED		LONGITUDINAL SECTION	
APPROVED		JOB PIECE NO. 28036(04)	SHEET NO. 85
SQUAD	G/K ENGR.		

X:\quernsey\EC-1360\Struc\Bridge\_C\LONG.dgn 6/2/2016